

APPENDIX E: PIC Displays



ONLINE PUBLIC INFORMATION CENTRE 1



hwy401eastofbrockville.ca

Highway 401

East of Brockville

Planning, Preliminary Design, and
Class Environmental Assessment Study

GWP 4111-22-00



Welcome to

Online Public Information Centre 1

The purpose of this online PIC is to present and gather your feedback on:

- Project background and the process being followed
- Transportation needs and opportunities in the study area
- The alternative bridge and culvert rehabilitation and replacement strategies, and the future footprints for the Highway 401 corridor that will be evaluated as part of this project
- Preliminary existing study area conditions

Other information related to this PIC that is available on the project website includes:

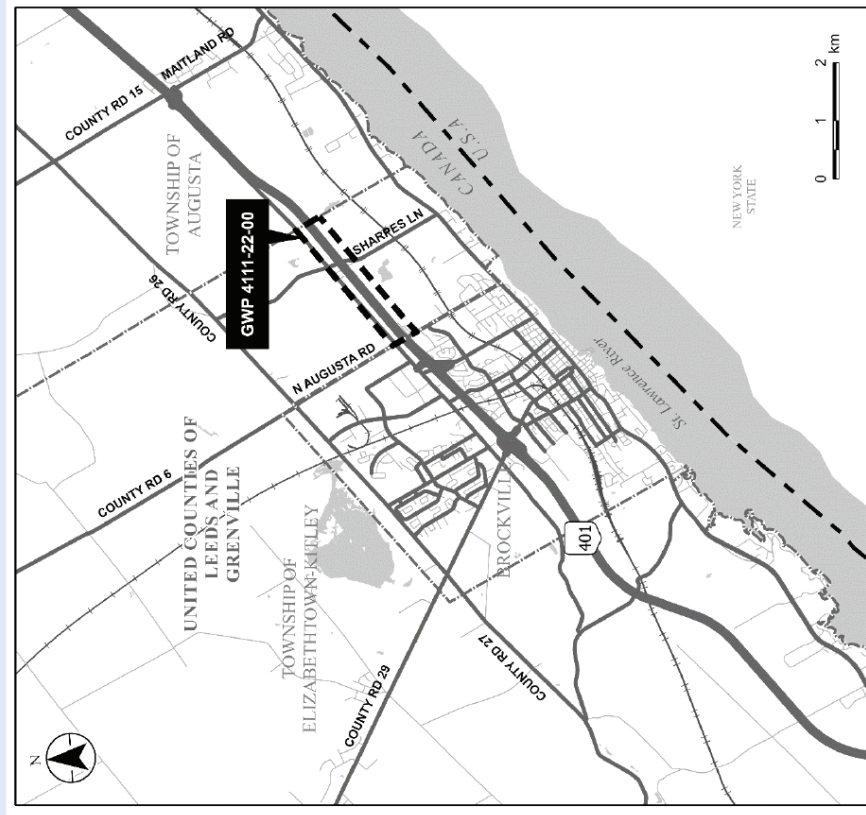
- A PDF copy of this presentation
- A PDF copy of the study area
- A PDF copy of the preliminary alternatives under consideration
- Online comment form

About The Project

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of Sharpe's Lane bridge and Butler Creek culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

Highway 401 is a controlled-access 400-series provincial highway that connects southwestern Ontario (Windsor) to Quebec, a total of 830 km. The highway, and the structures along it, were built in the 1950s and 1960s.

As part of this study, the project team will be reviewing existing conditions, developing and evaluating alternatives, identifying appropriate improvements, and developing environmental protection and mitigation measures. At the completion of the study, a recommended plan for the Highway 401 study area will be confirmed and designated (i.e. protected).



Class Environmental Assessment Process Group 'B' Projects

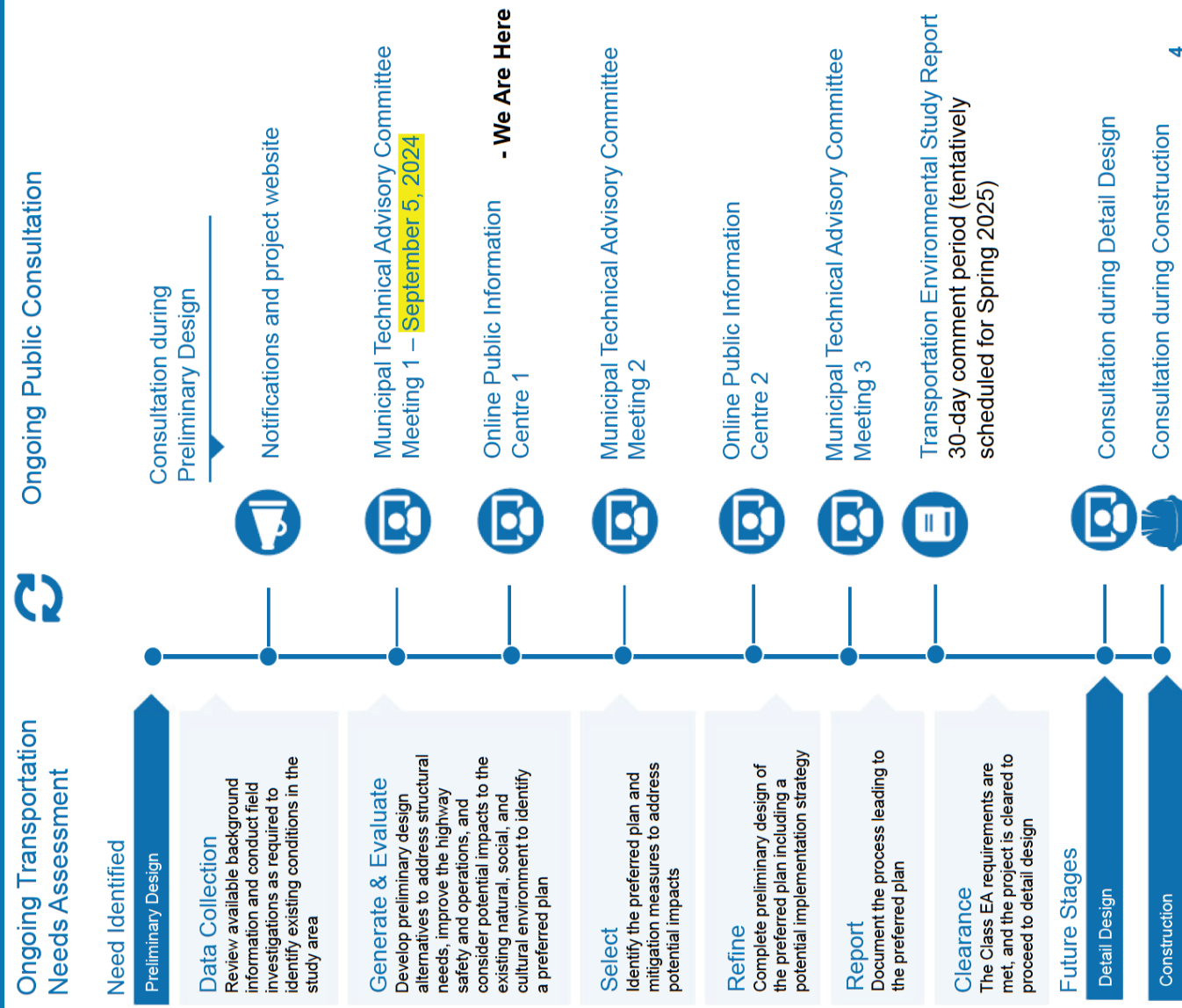
Environmental Assessment Process

This study is being carried out under the requirements of the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024), which is approved under the Ontario Environmental Assessment Act for provincial transportation projects of a defined scope and magnitude.

The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process, which includes major improvements to existing provincial transportation facilities.

At the end of the study, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day comment period.

[Please click here to access a copy of the MTO Class EA document](#)



Problem And Opportunities

The purpose of the study is to identify a recommended plan that addresses current and future transportation needs in the study areas as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network. This study will include reviewing existing conditions, developing and evaluating alternatives, identifying a preferred plan, and developing environmental protection/mitigation measures.

A Recommended Plan will be confirmed and designated (i.e., protected) at the completion of the study.

This study has been initiated to address the following problem and opportunities:

Problem

- The bridge and structural culvert in the study area are nearing the end of their service life and will require rehabilitation and/or replacement in the near future
- The existing Highway 401 platform cannot accommodate the traffic staging required to rehabilitate or replace the bridge and structural culvert

Opportunities

- Develop appropriate rehabilitation or replacement strategies to maintain the safe operation of the highway corridor for the current and future planning horizons
- Identify the ultimate footprint for the Highway 401 corridor to address current and future transportation needs

5

Alternatives To The Undertaking

The Class EA requires that 'reasonable alternatives' be considered to address identified problems. This involves two levels of analysis. The Alternatives to the Undertaking considers a broad range of alternatives that could address the project needs. Once the best alternative is selected, the Alternative Methods of Carrying out the Undertaking can be studied.

The Alternatives to the Undertaking identified for this study are listed below.

Alternatives to the Undertaking	Does it address the problems?
<p>Do Nothing Rehabilitate existing bridge and culvert with no accommodation for the Highway 401 footprints for interim six and ultimate eight lanes.</p>	<p><input checked="" type="checkbox"/> Does not address structural replacement needs and anticipated future growth needs. Do not carry forward.</p>
<p>Transportation Demand Management (TDM) TDM shifts demand on the highway network by shifting demands to the time periods outside of the critical congestion periods and shifts demand to alternative modes of transportation.</p>	<p><input checked="" type="checkbox"/> Does not address structural replacement needs. Do not carry forward.</p>
<p>Improve Adjacent Road Systems Expansion of existing municipal and regional road networks.</p>	<p><input checked="" type="checkbox"/> Does not address structural replacement needs. Do not carry forward.</p>
<p>Improved Provincial Transportation Facility Replace existing bridge and culvert to accommodate the Highway 401 footprints of interim six and ultimate eight lanes.</p>	<p><input checked="" type="checkbox"/> Addresses structural replacement needs and anticipated growth needs. Carry forward.</p>

6

Project Overview



Bridge Improvement

The Sharpe's Lane bridge is approaching the end of its service life and will need to be rehabilitated or replaced. The bridge will need to accommodate the future Highway 401 footprint for interim six lanes and ultimate eight lanes.

> for more details, see:

Bridge Improvement Alternatives

S Sharpe's Lane bridge over Highway 401



Highway 401 Improvements

The study includes establishing the future Highway 401 footprint for the interim six lanes and ultimate eight lanes configurations.

> for more details, see:

Highway 401 Alternatives



Drainage Improvements

There is a structural culvert at Butler Creek which is approaching the end of its service life and will need to be rehabilitated or replaced. The culvert will need to accommodate the future Highway 401 footprint for interim six lanes and ultimate eight lanes. Existing structural and non-structural culverts are shown on the plan above.

> for more details, see:

Drainage Improvements

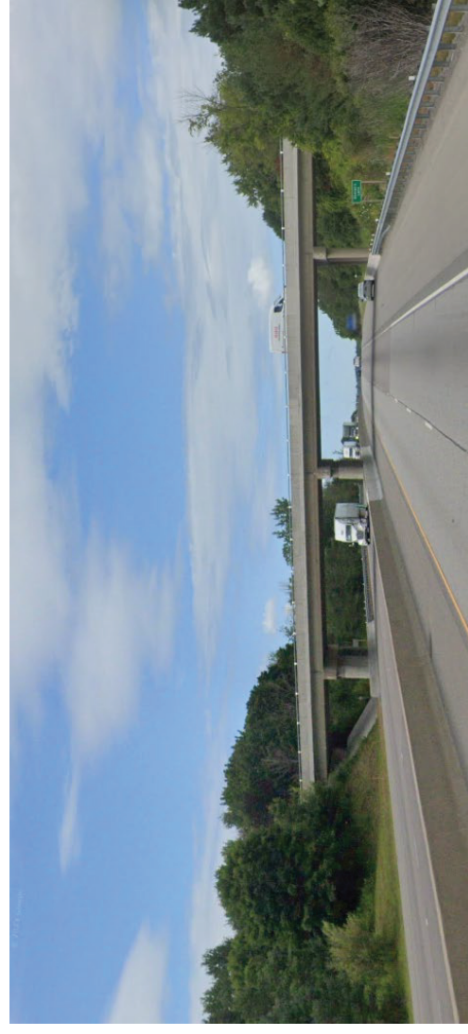
B Butler Creek Culvert

LEGEND

- Existing Highway 401 right-of-way
- Existing Structural Culvert
- Existing Non-Structural Culvert
- Existing Noise Barrier Wall

[Please click here to review a high-resolution version of the Project Overview plan](#)

Existing Structure Conditions



Sharpe's Lane - Site 16X-0125/B0

- Spans over 4 lanes of Highway 401
- Accommodates 2 lanes of traffic on Sharpe's Lane
- Constructed in 1967 and last rehabilitation in 2017
- Structure in overall good condition
- Does not accommodate the future Highway 401 footprint for an interim six lanes and ultimate eight lanes



Butler Creek Culvert – Site 16X-0238/C0

- Rigid frame structure
- Accommodates 4 lanes of Highway 401
- Constructed in 1959
- Structure in fair condition with several areas of deterioration
- Does not accommodate the future Highway 401 footprint for an interim six lanes and ultimate eight lanes

Preliminary Structure Alternatives

Sharpe's Lane

Bridge Improvement Alternatives



1

Replace an existing alignment



2

New alignment - West



3

New alignment - East

A range of alternatives for replacing this bridge have been developed. A preliminary screening has been carried out to identify the alternatives that should be carried forward for further consideration.

Carried-forward ✓
Screened-out ✗



Bridge closed with detour

Advantages

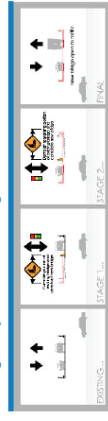
- Retains existing alignment of Sharpe's Lane
- Less property required compared to Alternatives 2 and 3
- Faster method of construction compared to staged construction with single lane
- Lower construction staging cost compared to keeping bridge open during construction

Disadvantages

- Impacts 2 hydro poles
- Introduces up to 8 km of travel for traffic to cross Highway 401 via detour route

Note: The final detour route will be confirmed in consultation with the affected municipalities.

Bridge open with single lane



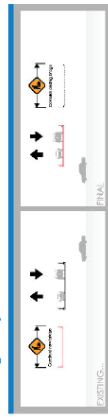
Advantages

- Retains existing alignment of Sharpe's Lane
- Maintains access across Highway 401 during construction
- Less property required compared to Alternatives 2 and 3

Disadvantages

- Impacts 2 hydro poles
- Minor traffic delays due to single lane of traffic across the bridge during construction
- Longer construction duration compared to closing the bridge
- Higher construction staging cost compared to closing the bridge

Bridge open with two lanes



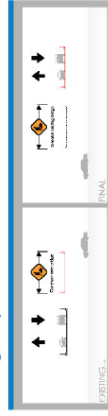
Advantages

- Maintains access across Highway 401 during construction with minimal impacts to traffic
- Similar construction duration as closing the bridge
- No hydro pole impacts

Disadvantages

- Requires alignment shift on Sharpe's Lane
- Requires more property compared to Alternatives 1 and 3
- Significantly higher cost compared to replacing on existing alignment

Bridge open with two lanes



Advantages

- Maintains access across Highway 401 during construction with minimal impacts to traffic
- Similar construction duration as closing the bridge
- Similar construction staging cost as closing the bridge

Disadvantages

- Requires alignment shift on Sharpe's Lane
- Results in undesirable horizontal curve on bridge
- Requires more property compared to Alternative 1
- Impacts 3 hydro poles
- Significantly higher cost compared to replacing on existing alignment

Do Nothing

Disadvantages

- Does not accommodate short-term or long-term structure needs

LEGEND

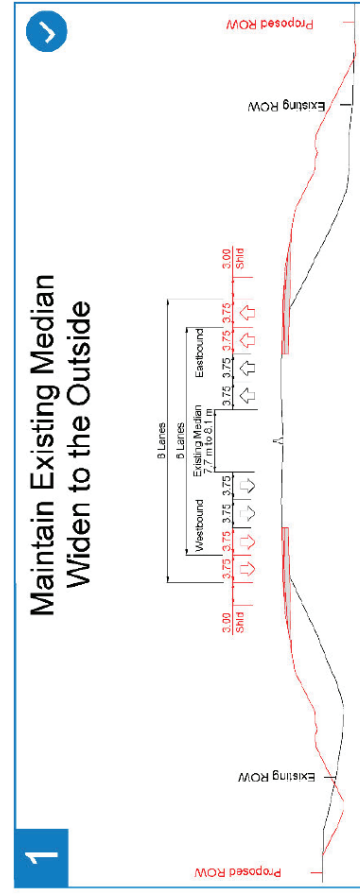
- New Roadway
- New Structure
- Property Required
- Limit of Existing WTO Right-of-way

[Please click here to review a high-resolution version of Sharpe's Lane Alternatives](#)

Highway 401 Alternatives



Cross-Section Improvement Alternatives



1

Maintain Existing Median Widen to the Outside

Advantages

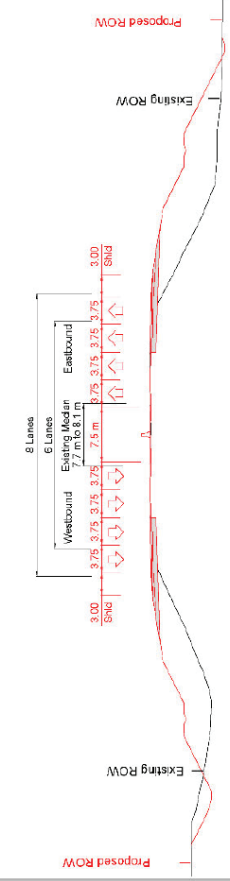
- Retains the existing median with concrete barrier
- Minimizes impacts to traffic during construction
- Lower cost compared to Alternative 2

Disadvantages

- Requires grading outside of the existing highway footprint
- Requires more property and potential environmental impacts compared to Alternative 2

2

Standard Median (7.5 m) Widen to the Inside and the Outside



Advantages

- Provides standard median width with concrete barrier (7.5 m)
- Fewer property and potential environmental impacts compared to Alternative 1

Disadvantages

- Requires more complicated staging compared to Alternative 1
- Requires reconstruction of the median including concrete barrier
- Higher cost compared to Alternative 1

LEGEND

- Existing roadway
- New roadway

[Please click here to review a high-resolution version of the Highway 401 Alternatives](#)

Drainage Improvements



There is one structural culvert in the study area.
The following improvement strategies will be considered:

Replace with new culvert:

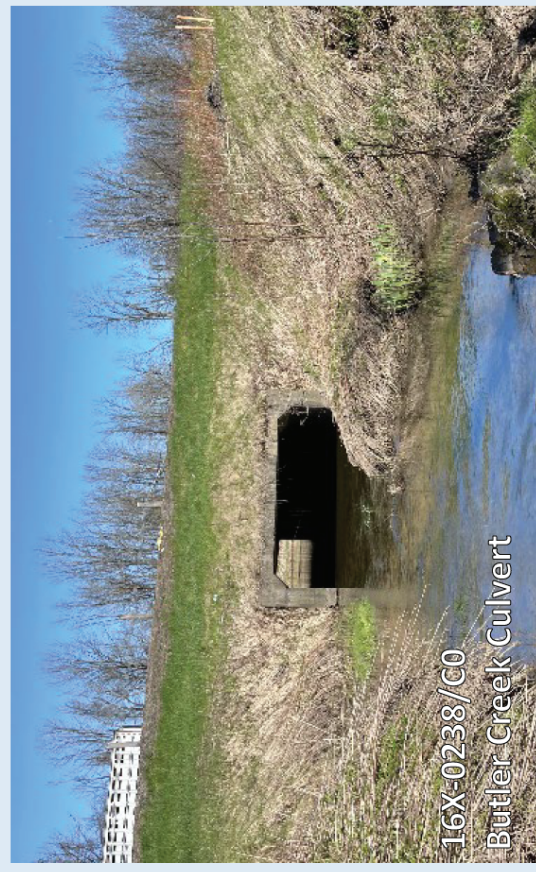
- + Long-term strategy
- + Accommodates interim (6-lane) Highway 401
- + Accommodates ultimate (8-lane) Highway 401
- Requires significant excavation

Culvert rehabilitation and retaining walls:

- Not a long-term strategy
- + Potential short-term strategy
- + Accommodates interim (6-lane) Highway 401
- Does not accommodate ultimate (8-lane) Highway 401
- Requires retaining walls

Culvert rehabilitation with extension:

- Not a long-term strategy
- + Potential short-term strategy
- + Accommodates interim (6-lane) Highway 401
- Does not accommodate ultimate (8-lane) Highway 401
- Future replacement of original culvert can be challenging



Evaluation Process



Identify Criteria

Evaluation Criteria are established through:

- public input
- similar projects
- provincial guidelines
- existing conditions



Weigh Criteria

Each criterion is assigned a weight factor that best reflects its relative importance.



Evaluate Alternatives

All feasible alternatives are measured and given a score based on how well each is judged to satisfy the evaluation criteria



Rank Alternatives

The sum of the weighted scores provides a total score for each alternative. This is the basis for ranking the alternatives and identifying the Preferred Plan

How is the Recommended Plan Selected?

A detailed evaluation of alternatives will be carried out to identify an improvement plan that addresses structural and future transportation needs and provides safe operations, while minimizing the impacts to the natural, social and cultural environments. This is accomplished by identifying evaluation criteria along with their relative importance, and then ranking the overall scores of the design alternatives.

A second PIC will be scheduled following PIC 1 to present the evaluation of the alternatives and a preferred plan for the Highway 401 Planning Study.

The Preferred Plan

The concluding step in the analysis and evaluation process is the selection of a preferred plan.

This process includes:

- Reviewing the results of the analysis and evaluation based on specialist work and input received during the study
- Determining which criteria have the most influence on the outcome of the evaluation process
- Considering the sensitivity of the weightings
- Confirming the ranking of the alternatives
- Considering public/stakeholder response to the evaluation process

Preliminary Evaluation Criteria

Engineering

- Geometrics & Safety**
- Design standards for provincial highways
 - Potential for collisions on Highway 401
 - Crossing road grades
 - Pedestrian and cyclist accommodations

Constructability

- Construction techniques
- Traffic flow and operations, including local access and out-of-way travel

Utilities

- Impacts to utilities
- Cost**
- Consider total cost including utility relocations and property acquisition

Community

- Property**
- Impacts to private property
- Noise & Air Quality**
- Proximity to residences
- Built & Cultural Heritage**
- Impacts to potential cultural heritage resources

- Archaeology**
- Impacts to areas of archeological potential

Contamination

- Potential to encounter contaminated soils/groundwater

Environment

Terrestrial Ecosystem

- Impacts on wildlife habitat
- Impacts on significant trees and vegetation

Fish & Fish Habitat

- Impacts to watercourses and fish and fish habitat

Species of Conservation Concern

- Impacts to Species-at-Risk and habitat associated with Species-at-Risk

Designated Areas

- Impacts to Significant Ecological Area



Bridge Improvements



Culvert/Drainage Improvements

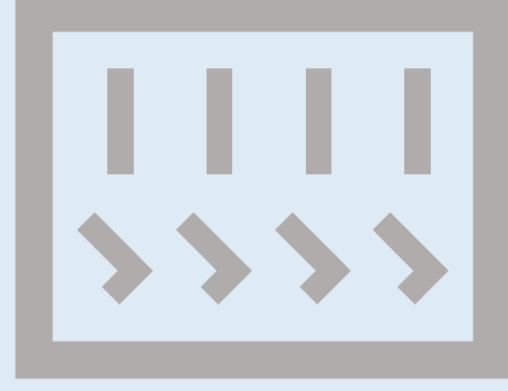


Highway Improvements

13

Next Steps

The following will be completed following this PIC:




- ✓ Review, consider and respond to the comments received
- ✓ Evaluate alternatives
- ✓ Identify the Preferred Plan
- ✓ Identify the potential impacts and mitigation measures
- ✓ Host second Online PIC February 2025
- ✓ Prepare the Transportation Environmental Study Report (TESR)
- ✓ Issue Notice of Study Completion and File the TESR for a 30-day comment period (Spring 2025)

hwy401eastofbrockville.ca

Thank you for participating in this online PLC

Ways to provide your comments:

-  Complete the Online Survey
-  Email comments@hwy401eastofbrockville.ca
-  Contact by telephone:

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Visit the Study website:
<http://www.hwy401eastofbrockville.ca>

We would appreciate receiving your comments by October 16, 2024



Freedom of Information and Protection of Privacy Act
Comments and information regarding this study are being collected to satisfy the requirements of the Ontario Environmental Assessment Act, and in accordance with the Freedom of Information and Privacy Act. With the exception of personal information, all comments will become part of the public record.

Highway 401 East of Brockville, Planning, Preliminary Design, and Class EA – Online Public Information Centre 1 Presentation Script

Slide 1 (Introduction)

Hello, and thank you for joining us for this online Public Information Centre for the Planning, Preliminary Design, and Class Environmental Assessment of Highway 401 East of Brockville.

This video presentation will provide you with an overview of this important Class Environmental Assessment (or Class EA) study.

Slide 2 (Purpose)

This is the first Public Information Centre (PIC) for this study. A copy of the information presented will be available for your review on the project website, www.hwy401eastofbrockville.ca.

The purpose of this online PIC is to present and gather your feedback on:

- Project background and the process being followed
- Transportation needs and opportunities in the study area
- The alternative bridge replacement and culvert rehabilitation or replacement strategies, and the future footprint for the Highway 401 corridor that will be evaluated as part of this project
- The existing study area conditions

This online PIC also aims to answer questions you may have about this study.

Additional resources are available for you to download on the project website so that you can take the time to review the information in more detail. Comments can also be submitted on the project website.

Slide 3 (About the Project)

The Ontario Ministry of Transportation has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of Sharpe's Lane Bridge and Butler Creek Culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 750 m east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 kilometers), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

Highway 401 is a controlled-access, 400-series provincial highway that connects southwestern Ontario (Windsor) to Quebec, a total of 830 km. The highway, and the structures along it, were built in the 1950s and 1960s.

As part of this study, the project team will be reviewing existing conditions, developing and evaluating alternatives, identifying appropriate improvements, and developing environmental protection and mitigation measures. At the completion of the study, a recommended plan for the Highway 401 study area will be confirmed and designated (in other words, protected for future improvements). This study builds off of previously completed studies on Highway 401 adjacent to this study area and aligns with the future highway footprint identified as part of those adjacent

studies. Additional information for those studies is available at: www.highway401brockville.ca and www.highway401prescottmaitland.ca

Slide 4 (EA Process)

This study is being carried out under the requirements of the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024), which is approved under the Ontario Environmental Assessment Act for provincial transportation projects of a defined scope and magnitude.

The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process, which includes projects that modify access or add capacity to existing provincial transportation facilities, and new service, maintenance, and/or operations facilities, such as new interchanges, intersections or roundabouts, and highway and freeway expansions.

In 2019, modernization of the MTO Class EA (2000) was initiated to optimize the Class EA process by removing redundancies and expediting planning to facilitate a more streamlined implementation process. Amendments to the Class EA were approved in December 2023 and February 2024, and Class EA was renamed the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024). As part of the amendments, the EA process is now complete at the end of Preliminary Design, following the 30-day comment period for the Notice of Completion.

As part of the new MTO Class EA (2024), Preliminary Design is the primary opportunity to provide feedback on the project. Opportunities for meaningful consultation in future stages of the project will be minimal.

At the end of the study, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day comment period.

The goal of projects or activities covered under this Class EA is to provide a safe and effective transportation system while avoiding or minimizing negative environmental effects. The Class EA process consists of two main stages: Planning and Preliminary Design. The Planning stage focuses on developing a plan for how the project should be done, and the Preliminary Design stage focuses on 'roughing out' a design.

There are a number of steps to be completed as part of the Planning and Preliminary Design phases of the project, which allow for structured decision making to occur as the study progresses. These steps include:

- Data Collection – where available background information is reviewed, and field investigations are conducted as required to identify existing conditions in the study area
- Generating and evaluating alternatives – at this stage, Preliminary Design alternatives are developed to address structural needs, improve the highway safety and operations, and consider potential impacts to the existing natural, social, and cultural environment to identify a preferred plan
- Selection of a preferred plan – this step includes the identification of a preferred plan and proposes mitigation measures to address potential impacts to the environment
- Refine the preferred plan – as part of this step, the Preliminary Design of the preferred plan, including an implementation strategy, is proposed

- Reporting – once the information from the steps completed above have been gathered, considered, and finalized, the entire process is documented within a Transportation Environmental Study Report, or TESR, which will be posted on the project website for a 30-day public comment period. Notification of TESR Completion will be posted on the project website, within the *Brockville Recorder and Times* and *Brockville This Week* newspapers, and delivered via mail/email to those who have expressed an interest in this project
- Process Completion – this is an internal process that MTO uses to help to ensure that the Class EA requirements have been met before subsequent stages including detail design and construction begin

Consultation with the public, agencies, municipalities, and Indigenous communities, as well as identifying environmental protection measures are elements of the Planning and Preliminary Design phase of the project. Opportunities for consultation during this study are shown on this slide and include Public Information Centres, Municipal Technical Advisory Committee Meetings, responses to public notices, comments via the project website, and comments on the Transportation Environmental Study Report during the 30-day comment period.

The Implementation stage of the project, which includes Detail Design and construction of the proposed improvements, is not part of the Class EA process and will be undertaken in the future. As noted previously, opportunities for meaningful consultation during this stage of the project will be minimal as the Implementation stage is not part of the Class EA process. The timing of the Implementation stage of the project is not yet known.

Slide 5 (Problems and Opportunities)

The purpose of the study is to identify and recommend a plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network. This study will include reviewing existing conditions, developing and evaluating alternatives, identifying a preferred plan, and developing environmental protection/mitigation measures.

At the completion of the study, a Recommended Plan for Highway 401 will be confirmed and designated (i.e., protected). The Ministry will be registering a designation of a proposed highway plan based on the future needs determined by this Planning and Preliminary Design study. A designation is a mechanism for the Ministry to provide route/corridor protection as well as impose development control measures along the corridor and on adjoining owners. This type of designation is most often used where lands are still under private ownership, sometimes years in advance of intended property acquisition and construction. The study has been initiated to address the following problems and opportunities:

Problems

- The bridge and structural culvert in the Study Area are nearing the end of their service life and will require rehabilitation and/or replacement in the near future.
- The existing Highway 401 platform cannot accommodate the traffic staging required to rehabilitate or replace the bridge and structural culvert

Opportunities

- Develop appropriate rehabilitation or replacement strategies to maintain the safe operation of the highway corridor for the current and future planning horizons
- Identify the ultimate footprint for the Highway 401 corridor to address current and future transportation needs

Slide 6 (Alternatives to the Undertaking)

The Class EA requires that 'reasonable alternatives' be considered to address identified problems. This involves two levels of analysis. The Alternatives to the Undertaking considers a broad range of alternatives that could address the project needs. Once the best alternative is selected, the Alternative Methods of Carrying out the Undertaking can be studied.

The Alternatives to the Undertaking identified for this study include:

- Do Nothing, which includes the rehabilitation of the existing bridge and culvert with no accommodation for the future Highway 401 footprints for interim six and ultimate eight lanes
- Transportation Demand Management, or TDM, changes the demand on the highway network by shifting demands to times outside of the critical congestion periods and towards alternative modes of transportation
- Improve Adjacent Road Systems includes the expansion of the existing municipal and regional road networks
- Improved Provincial Transportation Facility includes the replacement of the existing bridge and culvert to accommodate the Highway 401 footprints of interim six and ultimate eight lanes

Based on the findings of the assessment of the Alternatives to the Undertaking, the Improved Provincial Transportation Facility alternative addresses the structural and anticipated growth needs, and is carried forward.

Slide 7 (Project Overview)

As part of this study, bridge, drainage, and Highway 401 improvements are being considered. Sharpe's Lane Bridge is approaching the end of its service life and will need to be replaced. The bridge will need to accommodate the future Highway 401 footprints for the interim six and ultimate eight lanes.

In addition, the Butler Creek Culvert beneath Highway 401 is approaching the end of its service life and will need to be rehabilitated or replaced. The culvert will also need to accommodate the Highway 401 footprint for interim six and ultimate eight lanes.

Lastly, the study includes establishing the future Highway 401 footprints for the interim six lane and ultimate eight lane configurations.

Slide 8 (Existing Structure Conditions)

The existing condition of the two structures within the Study Area have been reviewed and assessed. A summary of the findings of this assessment is shown on this slide. Sharpe's Lane Bridge was constructed in 1967 and was last rehabilitated in 2017. The bridge is in overall good condition; however, it does not accommodate the future Highway 401 footprint for interim six

and ultimate eight lanes. Butler Creek Culvert was constructed in 1959 and is in fair condition with several areas of deterioration. The culvert also does not accommodate the future Highway 401 footprint for interim six and ultimate eight lanes.

Slide 9 (Preliminary Structure Alternatives)

As noted, this study includes the preliminary design for modifications to Sharpe's Lane Bridge.

A range of alternatives for replacing this bridge have been developed, including replacement of the bridge on its existing alignment, on a new alignment to the west, and on a new alignment to the east. A preliminary screening of these and the 'Do Nothing' alternative was carried out, and it was determined that the bridge replacement alternatives should be carried forward for further consideration. The 'Do Nothing' alternative was not carried forward as it does not accommodate the short-term or long-term structure needs.

Slide 10 (Highway 401 Alternatives)

Two alternatives for improvements to Highway 401 were developed. Alternative 1 includes maintaining the existing median and widening the highway to the outside, and Alternative 2 includes a standard median and widening the highway to both the inside and outside.

The preliminary screening determined that only Alternative 1 should be carried forward as it retains the existing median and concrete barrier, minimizes impacts to traffic during construction, and has a lower cost compared to Alternative 2. Alternative 2 was screened out as it requires more complicated traffic staging compared to Alternative 1, requires reconstruction of the median including concrete barrier, and has a higher cost compared to Alternative 1.

Slide 11 (Drainage Improvements)

Butler Creek Culvert, Site 16X-0238/C0, is the only structural culvert in the Study Area. Three improvement strategies are being considered for the existing culvert, including replacement with a new culvert, culvert rehabilitation and construction of retaining walls, and culvert rehabilitation with a culvert extension. Both rehabilitation options are not considered long-term strategies as they do not accommodate the ultimate 8-lane Highway 401 configuration; however, they will both accommodate the interim 6-lane Highway 401 configuration. Replacement of the existing culvert with a new culvert will accommodate both the interim 6-lane and ultimate 8-lane Highway 401 configuration but requires significant excavation. All of the improvement strategies will be carried forward for further consideration.

Slide 12 (Evaluation Process)

A detailed evaluation of alternatives will be carried out to identify an improvement plan that addresses structural and future transportation needs and provides safe operations, while minimizing the impacts to the natural, social, and cultural environments. This is accomplished by identifying evaluation criteria along with their relative importance, and then ranking the overall scores of the design alternatives.

A second PIC will be scheduled following PIC 1 in order to present the evaluation of the alternatives and a Technically Preferred Plan for this Highway 401 Planning Study.

The concluding step in the analysis and evaluation process is the selection of a Technically Preferred Plan. This process includes:

- Reviewing the results of the analysis and evaluation based on specialist work and input received during the study
- Determining which criteria have the most influence on the outcome of the evaluation process
- Considering the sensitivity of the weightings
- Confirming the ranking of the alternatives
- Considering public/stakeholder response to the evaluation process

Slide 13 (Preliminary Evaluation Criteria)

The evaluation process includes identifying evaluation criteria through the input received during this study, the project team's experience in projects of this nature, provincial guidelines, and existing study area conditions.

Preliminary criteria have been developed and are presented on this slide for your review. These include:

- Engineering criteria, including Geometrics and Safety; Constructability; Utilities; and Cost considerations
- Community related criteria, including Property; Noise & Air Quality; Built & Cultural Heritage; Archaeology; and Contamination considerations
- Environmental criteria, including Terrestrial Ecosystem; Fish & Fish Habitat; Species of Conservation Concern; and Significant Groundwater Recharge Areas

Following this PIC, the evaluation criteria will be confirmed and assigned a weight factor. A comparative analysis of transportation benefits and environmental effects will be completed for each alternative.

The Technically Preferred Plan is selected as the aggregate of Preliminary Design alternatives that achieve the best overall balance of transportation engineering, individual environmental factor impacts, and overall environmental impact, taking into consideration the net environmental effects by applying conceptual mitigation measures.

Comments received from the public, Indigenous Communities, agencies, and other stakeholders will also be considered as part of the evaluation process.

Slide 14 (Next Steps)

The Project Team will complete the following after this Public Information Centre:

- Review, consider, and respond to the comments received
- Evaluate alternatives
- Identify the Technically Preferred Plan
- Identify the potential impacts and mitigation measures
- Host the second Online Public Information Centre for the project, which is tentatively scheduled for Spring 2025
- Prepare the Transportation Environmental Study Report for the project
- Issue a Notice of Study Completion and file the Transportation Environmental Study Report for a 30-day comment period, which is tentatively scheduled for Spring/Summer 2025

Slide 15 (Thank you)

On behalf of the Ontario Ministry of Transportation, we would like to thank you for your interest in this study, and for taking the time to participate in this online Public Information Centre. Your input is very important to us, and there are several ways to provide your comments.

We encourage you to review all PIC materials available on the project website, as well as to participate in the survey offered at the end of this presentation.

If you have any comments or questions, please do not hesitate to contact the project team via email at: comments@hwy401eastofbrockville.ca, or by contacting one of the project team members listed on this slide by telephone.

We would appreciate receiving any comments or questions you may have by November 8th, 2024. Thank you again for taking the time to participate in this online PIC.

**Highway 401 Planning Study
East of Brockville
ONLINE PUBLIC INFORMATION
CENTRE 1**

SUMMARY REPORT

GWP 4111-22-00



Prepared for:
Ministry of Transportation
Eastern Region
1355 John Counter Boulevard
Kingston ON K7L 5A3

Prepared by:
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek, ON L8J 0B4

November 27, 2024

Table of Contents

1.0 Introduction..... 3
2.0 Purpose 3
3.0 Location, Date, and Time 3
4.0 Notification..... 3
5.0 Reference Materials and Handouts 4
6.0 Format 5
7.0 Participation..... 5
8.0 Comments Received 6
9.0 Next Steps..... 9

List of Tables

Table 1: Statistics Gathered from Project Website 5
Table 2: Public Input Received and Responses Provided/Action Taken..... 6

List of Appendices

APPENDIX A Notification Materials 10
APPENDIX B PIC 1 Materials21
APPENDIX C Online Participation Statistics.....46
APPENDIX D Comments Received48
APPENDIX E Indigenous Engagement74
APPENDIX F Newspaper Articles.....87



1.0 Introduction

The Preliminary Design and Class Environmental Assessment (Class EA) Study for Highway 401 East of Brockville has two planned Public Information Centres (PIC). The first of two PICs took place from October 2, 2024, to November 8, 2024, and was held in an online format on the project website. A recorded presentation and comment forms were provided on the website as part of the online PIC. Hard copies of the presentation were also available on request.

2.0 Purpose

PICs form part of the overall consultation plan for the project and are designed to involve stakeholders early and throughout the study to identify concerns and provide opportunities for input.

The purpose of PIC 1 was to provide the public and stakeholders with an opportunity to review the existing study area conditions, the preliminary improvement alternatives, the proposed evaluation criteria, evaluation process, and to comment on project activities to date.

A second PIC will be scheduled later in the study to present the results of the evaluation process, the Technically Preferred Plan, the anticipated impacts and proposed mitigation measures, and next steps in the study process.

3.0 Location, Date, and Time

PIC 1 consisted of a recorded presentation that was hosted on the study website (<http://www.hwy401eastofbrockville.ca>) beginning on October 2, 2024. Comments related to the online PIC were requested by November 8, 2024. Hardcopies of the presentation and transcript were available for mailout for those unable to access the PIC 1 materials provided on the study website.

4.0 Notification

The Notice of PIC 1 provided information about the PIC, including the purpose, and date and time that the presentation would become available on the study website. In addition, comments were requested from the public by November 8, 2024.



The notice and a letter were also sent via email to the MPP for Leeds-Grenville-Thousand Islands and Rideau Lakes on September 17, 2024, and to external agencies, businesses, and members of the public on September 25, 2024. A hard copy of the notice was also sent to 2,126 residents and business owners during the week of September 23, 2024, via Canada Post Admail.

Notices to advise the public of the first PIC were published in the following newspapers:

- *Brockville Recorder & Times* (in English, on October 3, 2024)
- *Brockville This Week* (in English, on October 3, 2024)

Letters were sent to the following Indigenous communities/organizations via email on September 18, 2024, to notify them of the first PIC:

- Métis Nation of Ontario
- Mohawks of Akwesasne

The Notice of PIC 1 and associated letters provided information about the PIC, including its purpose, the date and time that the presentation would become available on the study website.

Comments regarding the Online PIC presentation were requested to be submitted by November 8, 2024.

Copies of the newspaper notices and the notification materials are provided in **Appendix A**.

5.0 Reference Materials and Handouts

The following displays were presented as part of the online PIC 1 presentation:

- Welcome
- About the Project
- Class Environmental Assessment Process
- Problem and Opportunities
- Alternatives to the Undertaking
- Project Overview
- Existing Structure Conditions
- Preliminary Structure Alternatives
- Highway 401 Alternatives
- Drainage Improvements
- Evaluation Process
- Preliminary Evaluation Criteria



- Next Steps
- Thank you for participating

A copy of the PIC displays are included in **Appendix B**.

6.0 Format

As noted, the PIC was hosted online via the study website <http://www.hwy401eastofbrockville.ca>. A recorded presentation, including the transcript and displays, were available for review, and members of the public were encouraged to submit comments and questions or to contact the project team to provide feedback on the study. Hardcopies of the transcript and displays were available via courier to those unable to access the online format, where requested.

The information available as part of the online PIC, including a copy of the presentation, transcript, and comment forms is provided in **Appendix B**.

7.0 Participation

Statistics were gathered during the online PIC period from October 2, 2024, to November 8, 2024, to determine the number of viewers of the online presentation. The website statistics were broken down into the following two categories:

- 1) **Views** – The number of pageviews on a website and screenviews on an app. For this category, the same person visiting the website multiple times during the PIC time period can be counted multiple times.
- 2) **Active users** – The total number of users that accessed the website during the PIC period. For this category, the same person visiting the website multiple times during the PIC is counted once.

The data gathered for each category is illustrated in Table 1.

Table 1: Statistics Gathered from Project Website

Views	Active Users
45	32

A total of 32 users to the website were recorded during the PIC time period.

A copy of the raw statistics of the participants is provided in **Appendix C**.



8.0 Comments Received

In total, six (6) comments were received through email and three (3) comments were received through the website's online survey. The online survey was provided as a link within the PIC 1 presentation. Copies of the comments are presented in **Appendix D**. All personal information has been removed, consistent with the *Freedom of Information and Protection of Privacy Act*.

An overview of the comments received during the PIC comment period is provided in Table 2.

Table 2: Public Input Received and Responses Provided/Action Taken

Comment	Response Provided and/or Action Taken
Previous EA Highway 401 North Augusta Road and Stewart Boulevard	
<ul style="list-style-type: none"> • Questioning how North Augusta Road off ramp is going to accommodate left turns for driving over the overpass • How are we addressing North Augusta and Stewart Boulevard 	<p>Thank you for your comments and interest in the Highway 401 Planning Study East of Brockville Tom and Linda.</p> <p>The study area is from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road. A separate study was previously completed from 2 km west of Stewart Boulevard to 750 m east of North Augusta Road and additional information can be found at the project website: https://www.highway401brockville.ca/</p> <p>Your comments have been forwarded to the Brockville study team for a response.</p>
Requests to be Added to Project Mailing List	
<ul style="list-style-type: none"> • Asked to be included in the project mailing list 	<p>Thank you for your interest in the Highway 401 Planning Study East of Brockville. Your contact has been added to the mailing list, and you will receive updates on the project.</p>
Noise Barriers	



**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 1**

Comment	Response Provided and/or Action Taken
<ul style="list-style-type: none"> • Comment asking if a noise barrier is planned for areas like Old Sharps Lane • Worried for noise level along Waverly and Oxford Avenue 	<p>Thank you for your comments and interest in the Highway 401 Planning Study East of Brockville.</p> <p>The project team will be completing a Noise Impact Assessment as part of this study. This noise analysis will follow the Ministry of Transportation’s Environmental Guide for Noise (the MTO Guide) published in 2022 which complies with the requirements of the Ontario Environmental Assessment Act. This assessment will include a review of previous studies relevant to the area, along with an analysis of Noise Sensitive Areas (NSAs), proposed developments, traffic data, construction equipment and schedules, and any applicable local or municipal noise bylaws.</p> <p>The team will evaluate road traffic noise impacts carefully, and any proposed noise mitigation measures will be assessed for both technical and economic feasibility. Stantec will complete the Noise Impact Assessment Report for the technically preferred plan. Based on the results of this evaluation, the project team will also provide alternative mitigation options, ensuring that various approaches are considered to help minimize noise impacts effectively.</p> <p>Thank you again for your interest in this study.</p>
Other Comments	
<ul style="list-style-type: none"> • Inquired that the North Augusta Road labelling on the site map was incorrect 	<p>Thank you for your email and interest in the Highway 401 Planning Study East of Brockville.</p> <p>Thank you for your comment regarding the study area map on the project notice. Please note that due to map size restrictions on our project notice, we are not able to include all road labels and for this map we only included the North Augusta label north of Highway 401 and not south of Highway 401 to make sure that other roads were visible on the map.</p>



**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 1**

Comment	Response Provided and/or Action Taken
	<p>Your contact information has been added to the project mailing list and you will be notified of key project updates.</p>
<ul style="list-style-type: none"> • Lafarge employee requesting to be included in the project mailing list 	<p>Thank you for your email.</p> <p>Your contact information has been added to the project mailing list and you will be notified of key project updates.</p> <p>The project team for the above-referenced study is hoping to gather feedback and input on the Highway 401 Planning Study East of Brockville.</p> <p>The first online Public Information Centre (PIC) for this study is now available for review on the study website at: http://www.hw401eastofbrockville.ca . The purpose of PIC 1 is to display and seek input on the existing conditions in the study area (i.e. natural, social, economic, and cultural), present the preliminary alternatives and proposed evaluation criteria, and answer questions about the study. The presentation includes preliminary bridge improvement alternatives at Sharpe’s Lane Bridge.</p> <p>We encourage you to view the PIC 1 presentation and let us know if you have any questions or comments regarding the study and in particular the alternatives presented for Sharpe’s Lane Bridge and how they relate to operations at the Lafarge facility.</p>
<ul style="list-style-type: none"> • Encouraged that the ministry is taking precaution on safety and structural issues 	<p>Thank you for your interest in the Highway 401 Planning Study East of Brockville. Your comments have been shared with the project team, and your contact has been added to the mailing list for updates.</p>



9.0 Next Steps

Responses to all formal questions and comments received as a result of the first PIC were prepared and forwarded to the commenting party. Consultation is ongoing, and all relevant agencies will be notified at future project milestones.

Following PIC 1, the evaluation of alternatives will be undertaken to identify the Technically Preferred Plan. A second PIC is anticipated to be undertaken in Winter 2025. The purpose of the second PIC is to present and seek input on the evaluation of alternatives, the Preferred Plan, and proposed mitigation and protection measures, and to obtain feedback on the Preferred Plan. All comments received will be addressed and the Technically Preferred Plan will be updated, if required.

Following the second PIC, a Transportation Environmental Study Report (TESR) will be prepared to document the study and the Recommended Plan, and it will be made available for a 30-day public comment period. Any concerns brought to the attention of the Project Team will be responded to and an attempt to reach a resolution will be made.

APPENDIX A Notification Materials



MINISTRY OF TRANSPORTATION

Capital Planning and Program
Development East Office
Asset Management Branch
1355 John Counter Blvd
PO Box 4000
Kingston, ON K7L 5A3
Tel.: 613-893-3031
1-800-267-0295
Fax: 613-540-5106

MINISTÈRE DES TRANSPORTS

Prestation de programmes d'ingénierie de
l'Est
Direction de conception et d'ingénierie
1355, boulevard John Counter
Case postale 4000
Kingston (Ontario) K7L 5A3
Tél.: 613-893-3031
1-800-267-0295
Télééc.: 613-540-5106

**MINISTRY OF TRANSPORTATION**

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Kingston (Ontario) K7L 5A3
Tél.: 613-893-3031
1-800-267-0295
Télééc.: 613-540-5106



September 17, 2024

Mr. Steve Clark, MPP
Leeds-Grenville-Thousand Islands and Rideau Lakes
100 Strowger Blvd.
Suite 101
Brockville ON K6V 5J9
steve.clark@pc.ola.org

Dear Steve Clark:

**Reference: Highway 401 Planning Study East of Brockville (GWP 4111-22-00) -
Notice of Online Public Information Centre 1**

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and/or rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of this letter is to advise you of the first online Public Information Centre (PIC) planned for this study. As indicated within the attached notice, the online PIC will be available for your review on the study website at www.hwy401eastofbrockville.ca, starting on October 2, 2024. A recorded presentation will be available as part of the online PIC.

The information presented at this online PIC will provide you with an opportunity to review and comment on the existing study area conditions, identified challenges and opportunities, preliminary alternatives, and proposed evaluation criteria.

A copy of the Notice of Public Information Centre 1 is attached and will be placed in the Brockville Recorder and Times and Brockville This Week on October 3, 2024.

This project is being completed as a Group 'B' project under the MTO Class Environmental Assessment (Class EA) for Provincial Transportation Facilities and Municipal Expressways (2024). This study will include opportunities for public engagement, including two Public Information Centres. Future notices will be advertised in local newspapers and sent to the project mailing list.

Upon completion of Preliminary Design, a Transportation Environmental Study Report (TESR) will be prepared and made available for 30-day public comment period. Notices

will be published in local newspapers to advise the public of the TESR public comment period.

You are encouraged to visit the project website, www.hwy401eastofbrockville.ca to obtain additional study information and/or to submit comments to the study team. If you have any questions regarding the study, please contact the undersigned, or one of the individuals named in the attached notice.

Sincerely,

David Brake
Senior Project Manager, Ministry of Transportation
Capital Planning and Program Development East
1255 John Counter Boulevard, Postal Bag 4000
Kingston ON K7L 5A3
Tel: 613-893-3031

Attachment: Notice of Public Information Centre 1



**NOTICE OF PUBLIC INFORMATION CENTRE 1
PRELIMINARY DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT
Highway 401 Planning Study East of Brockville, GWP 4111-22-00**

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville. The purpose of the study is to identify a Recommended Plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network.

This notice is to announce the first online Public Information Centre (PIC) for this study. The purpose of PIC 1 is to display and seek input on the existing conditions in the study area (i.e. natural, social, economic, and cultural), present the preliminary alternatives and proposed evaluation criteria, and answer questions about the study.

The online PIC will be available for review on the study website at www.hwy401eastofbrockville.ca starting October 2, 2024. A recorded presentation will be available as part of the online PIC until November 8, 2024.

THE PROCESS

This study is a "Group B" project under the *Class Environmental Assessment (EA) for Provincial Transportation Facilities and Municipal Expressways (2024)* and includes undertaking environmental and engineering field investigations and seeking input from Indigenous communities, the public, stakeholders, municipalities and government agencies. Upon completion of preliminary design, a *Transportation Environmental Study Report (TESR)* will be prepared and made available for public comment. Notices will be published in local newspapers to advise the public of the TESR public comment period.

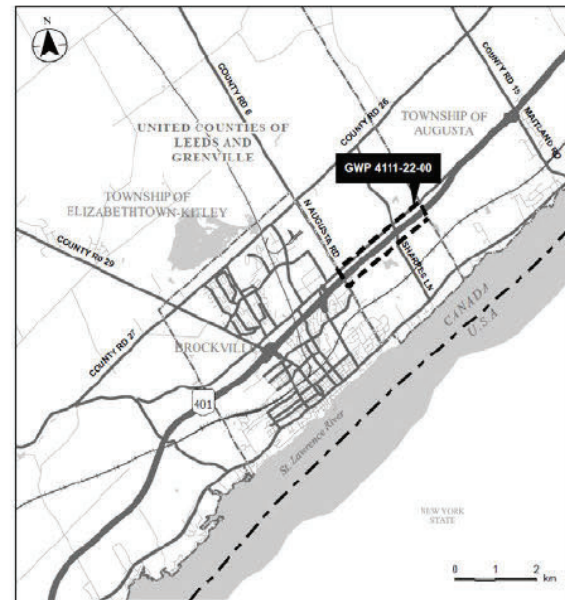
COMMENTS

You are encouraged to participate in the study and to provide comments in writing to the project team. If you wish to have your name added to the project mailing list, or have questions about the study, please contact:

Gregg Cooke, P.Eng.
Consultant Project Manager
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek ON L8J 0B4
Tel: (905) 381-3227
Call Collect: (905) 385-3234
comments@hwy401eastofbrockville.ca

David Brake
Senior Project Manager
Ministry of Transportation Eastern Region
1355 John Counter Boulevard, Postal Bag 4000
Kingston ON K7L 5A3
Tel: (613) 893-3031
Fax: (613) 540-5106
comments@hwy401eastofbrockville.ca

You are encouraged to visit the project website, www.hwy401eastofbrockville.ca, where project information including study notices, background information, PIC displays, and the TESR will be made available as the study progresses. You can also submit comments to the project team on the project website. If you have any accessibility requirements to participate in this project, please contact one of the Project Team members listed above. Comments and information are being collected to assist the MTO in meeting the requirements of the Ontario Environmental Assessment Act. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.



From: [Gazibara, Nevena](mailto:Gazibara_Nevena)
To: [Young, Rachel \(Waterloo\)](mailto:Young_Rachel)
Subject: FW: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00)
Date: Friday, November 29, 2024 11:24:45 AM
Attachments: [Notice of Public Information Centre 1 \(GWP 4111-22-00\).pdf](#)

Agency email (please save on server)

From: Vanhell, Shane <Shane.Vanhell@stantec.com>
Sent: Wednesday, September 25, 2024 10:38 AM
To: Dan.Minkin@ontario.ca; Erika.Leclerc@ontario.ca; karla.barboza@ontario.ca; Liam.Smythe@ontario.ca; tate.kelly@infrastructureontario.ca; noticereview@infrastructureontario.ca; tracy.adams2@ontario.ca; catherine.warren@ontario.ca; jon.orpana@ontario.ca; ldrynan@ektwp.ca; deputyclerk@ektwp.ca; planning@ektwp.ca; mczarny@ektwp.ca; jhutton@ektwp.ca; scassidy@ektwp.ca; ray.callery@uclg.on.ca; rick.kester@uclg.on.ca; shirley.white@uclg.on.ca; cherie.mills@uclg.on.ca; bill.guy@uclg.on.ca; chris.morrison@uclg.on.ca; sallen@brockville.com; mlocke@brockville.com; pwood@brockville.com; sdunbar@brockville.com; amcginnis@brockville.com; tbeaubiah@crca.ca; michael.barrett@parl.gc.ca; info@a2acollaborative.org; firechief@ektwp.ca; deputychief@ektwp.ca; fireadmin@ektwp.ca; rbowman@augusta.ca; atrickey@augusta.ca; Simon.Hardy@opp.ca; fireadmin@townshipleeds.on.ca; jeff.carss@uclg.on.ca; ron.ferguson@ucdsb.on.ca; director@cdsbeo.on.ca; bertrma@ecolecatholique.ca; transportation@steo.ca; pjrobertson@brockvillechamber.com; ann.weir@uclg.on.ca; karen.mcdowell@ontruck.org; canada.marketing@coachcanada.com; cathy.lennon@ofa.on.ca; tyler.brooks@ofa.on.ca; brutusdanis@aol.com; annber@xplornet.com; gchs@truespeed.ca; terry.white@lafarge.com; greg.odonnell@leasecocommercial.com; kerianne012@gmail.com; ldickson@cogeco.ca; john.maskell@group-works.com; Burns.j@sympatico.ca; Gvkossivas@rogers.com; Stockwellr@rogers.com; len.meilleur@gmail.com; icampbell8@hotmail.com
Cc: comments@hwy401eastofbrockville.ca
Subject: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00)

Good morning,

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and to identify the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of this email is to advise you of the first online Public Information Centre (PIC) planned for this study. As indicated within the attached notice, the online PIC will be available for your review on the study website at www.hwy401eastofbrockville.ca, starting on October 2, 2024. A recorded presentation will be available as part of the online PIC. The project team is requesting that comments be submitted by November 8, 2024.

The information presented at this online PIC will provide you with an opportunity to review and comment on the existing study area conditions, identified challenges and opportunities, preliminary alternatives, and proposed evaluation criteria.

You are encouraged to visit the project website, www.hwy401eastofbrockville.ca, to obtain current project information and to submit comments to the project team.

If you are unable to view the online PIC and would like further information regarding the study, please contact either the undersigned or one of the project team members named in the enclosed information.

Kind regards,

Shane Vanhell B. Sc.
Environmental Planner

Shane.Vanhell@stantec.com

Stantec
200-835 Paramount Drive
Stoney Creek ON L8J 0B4



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Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.



NOTICE OF PUBLIC INFORMATION CENTRE 1 PRELIMINARY DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT Highway 401 Planning Study East of Brockville, GWP 4111-22-00

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville. The purpose of the study is to identify a Recommended Plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network.

This notice is to announce the first online Public Information Centre (PIC) for this study. The purpose of PIC 1 is to display and seek input on the existing conditions in the study area (i.e. natural, social, economic, and cultural), present the preliminary alternatives and proposed evaluation criteria, and answer questions about the study.

The online PIC will be available for review on the study website at www.hwy401eastofbrockville.ca starting October 2, 2024. A recorded presentation will be available as part of the online PIC until November 8, 2024.

THE PROCESS

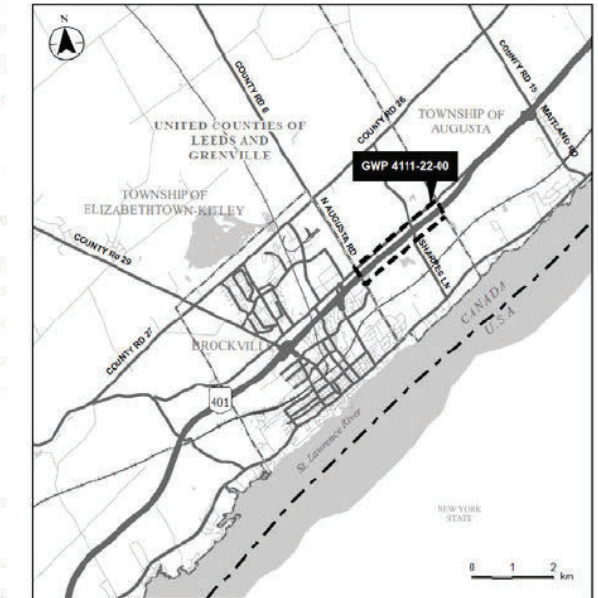
This study is a "Group B" project under the *Class Environmental Assessment (EA) for Provincial Transportation Facilities and Municipal Expressways (2024)* and includes undertaking environmental and engineering field investigations and seeking input from Indigenous communities, the public, stakeholders, municipalities and government agencies. Upon completion of preliminary design, a *Transportation Environmental Study Report (TESR)* will be prepared and made available for public comment. Notices will be published in local newspapers to advise the public of the TESR public comment period.

COMMENTS

You are encouraged to participate in the study and to provide comments in writing to the project team. If you wish to have your name added to the project mailing list, or have questions about the study, please contact:

Gregg Cooke, P.Eng.
Consultant Project Manager
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek ON L8J 0B4
Tel: (905) 381-3227
Call Collect: (905) 385-3234
comments@hwy401eastofbrockville.ca

David Brake
Senior Project Manager
Ministry of Transportation Eastern Region
1355 John Counter Boulevard, Postal Bag 4000
Kingston ON K7L 5A3
Tel: (613) 893-3031
Fax: (613) 540-5106
comments@hwy401eastofbrockville.ca



You are encouraged to visit the project website, www.hwy401eastofbrockville.ca, where project information including study notices, background information, PIC displays, and the TESR will be made available as the study progresses. You can also submit comments to the project team on the project website. If you have any accessibility requirements to participate in this project, please contact one of the Project Team members listed above. Comments and information are being collected to assist the MTO in meeting the requirements of the Ontario Environmental Assessment Act. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.

Highway 401 Widening - Brockville
 General Public - Project Mailing List - Notice of Study Commencement

Title	First Name	Last Name	Organization	Address	Address 2	City	Province	Postal Code	Telephone	Fax	Email
	Greg	O'Donnell	Leaseco			Mallorytown	ON		613-407-1013		greg.odonnell@leasecocommercial.com
	Ivan	Campbell		1451 County Road 2	RR#3	Brockville	ON	K6V 5T3	613-305-3064		
	Keri	Ryan									kerianne012@gmail.com
	Linda	Dickson		93 Sevenoaks Ave		Brockville					ldickson@cogeco.ca
	John	Maskell							613-341-7172		john.maskell@group-works.com
	Judy	Burns		114 Manor Drive		Brockville	ON	K6V 3E6			Burns.j@sympatico.ca
	Georgina	Kossivas									Gvkossivas@rogers.com
	Ross	Stockwell									Stockwellr@rogers.com
	Len	Meilleur		258 Adley Drive		Brockville	ON	K6V 7J2	250-516-2104		len.meilleur@gmail.com
	Alison	Storey									alisonastorey@icloud.com
	Mark	Kelly		344 Oxford Ave		Brockville			613-213-0350		m_kelly59@hotmail.com
	John	Williams									williamspsft@gmail.com
	Marg	Farmer		2208 Second Concession, RR1		Brockville	ON	K6V 5T1			billmargfarmer@yahoo.ca
	Sherlock	Holmes									sherlockholmes1945@yahoo.com
	Rebekka	Massie	Land Manager Eastern & Northern Ontario Lafarge Canada Inc	1651 Bearbrook Rd.		Ottawa	ON	K1B 1B8	343-550-2428		rebekka.massie@lafarge.com

Highway 401 Planning Study in the Township of Elizabethtown-Kitley (OWP 6111-22-60)
 External Agencies - Project Mailing List

External Agencies and Stakeholders	Title	First Name	Last Name	Position	Organization	Address	Address 2	City	Province	Postal Code	Telephone	Fax	Email
Federal Agencies													
		Ulmar	Naveed		CH Rail								Ulmar.Naveed@on.ca
Provincial Agencies													
		Dan	Minin	Heritage Planner	Ministry of Citizenship and Multiculturalism	401 Bay Street		Toronto	ON	M7A 6A7	416-786-7553		Dan.Minin@ontario.ca
		Elis	Ka	Heritage Planner	MCAM						416-305-0757		elis.k@mcam.on.ca
		Paul	Burns	Town Lead Heritage Planning Unit	MCAM						416-305-1077		paul.burns@mcam.on.ca
		Liam	Smythe	Heritage Planner	MCAM						416-301-4797		liam.smythe@mcam.on.ca
		Tim	Kelly	Planner	Infrastructure Ontario	1 Dundas Street West	Suite 2000	Toronto	ON	M5G 2L5	947-264-3819		tim.kelly@infrastructureontario.ca
		Ms. Catherine	Wright	Conservation Program Advisor	Ministry of Natural Resources and Forestry, Peterborough District	300 Water Street	1st Floor S	Peterborough	ON	K9J 8Z7	613-847-5116		c.wright@ontario.ca
		Mr.	Jan	Deputy Environmental Resource Planner & EA Coordinator	Ministry of Natural Resources and Forestry, Peterborough District	100 Water Street	2nd Floor S	Peterborough	ON	K9J 8Z7	705-772-0113		jan.wright@ontario.ca
		Mr.	Brandon	Norman	Management Biologist - Species at Risk Branch	135 St. Clair Ave W	7th Flr	Toronto	ON	M4V 1P5	613-561-8200		brandon.norman@ontario.ca
		Mr.	Brandon	Norman	Management Biologist - Species at Risk Branch	160 Bloor Street East	9th Floor	Toronto	ON	M7A 2E6	705-761-6880		brandon.norman@ontario.ca
		Mr.	Brandon	Norman	Management Biologist - Species at Risk Branch	160 Bloor Street East	9th Floor	Toronto	ON	M7A 2E6	705-761-6880		brandon.norman@ontario.ca
Municipal													
		Nick	Kickor	Chief Administrator	Township of Elizabethtown-Kitley	6544 New Dublin Rd		Addison	ON	R0E 1A0	613-345-7480 x 237		nick.kickor@etk.on.ca
		Laura	Stancel	Deputy Clerk	Township of Elizabethtown-Kitley	6544 New Dublin Rd		Addison	ON	R0E 1A0	613-345-7480 x 210		laura.stancel@etk.on.ca
		Barah	Hagen	Planning Administrative Assistant	Township of Elizabethtown-Kitley	6544 New Dublin Rd		Addison	ON	R0E 1A0	613-345-7480 x 217		barah.hagen@etk.on.ca
		Michael	Cooney	Planner	Township of Elizabethtown-Kitley	6544 New Dublin Rd		Addison	ON	R0E 1A0	613-345-7480 x 216		michael.cooney@etk.on.ca
		Michael	Cooney	Planner	Township of Elizabethtown-Kitley	6544 New Dublin Rd		Addison	ON	R0E 1A0	613-345-7480 x 216		michael.cooney@etk.on.ca
		Raymond	Calvin	Public Works Manager	Township of Elizabethtown-Kitley	6544 New Dublin Rd		Addison	ON	R0E 1A0	613-345-7480 x 216		raymond.calvin@etk.on.ca
		Rock	Kestler	CEO	United Counties of Leeds and Grenville	25 Central Ave. W	Suite 100	Brockville	ON	K6V 4N6	613-343-3840 x 2301		rock.kestler@ucg.on.ca
		Shane	White	Public Works Admin Assistant	United Counties of Leeds and Grenville	25 Central Ave. W	Suite 100	Brockville	ON	K6V 4N6	613-343-3840 x 2412		shane.white@ucg.on.ca
		Usher	Mills	Manager of Planning Services	United Counties of Leeds and Grenville	25 Central Ave. W	Suite 100	Brockville	ON	K6V 4N6	613-343-3840 x 2418		usher.mills@ucg.on.ca
		Bill	Clay	Manager of Engineering and Operations	United Counties of Leeds and Grenville	25 Central Ave. W	Suite 100	Brockville	ON	K6V 4N6	613-343-3840 x 2408		bill.clay@ucg.on.ca
		Chris	Morrison	Planning Manager	United Counties of Leeds and Grenville	25 Central Ave. W	Suite 100	Brockville	ON	K6V 4N6	613-343-3840 x 2403		chris.morrison@ucg.on.ca
		Mr.	Steven	Wilson	Supervisor of Engineering	1 Kings Street West	P.O. Box 5000	Brockville	ON	K6V 2A5	613-342-8772 x 3203		steven.wilson@etk.on.ca
		Matthew	Locks	Supervisor of Transportation Services	City of Brockville	1 Kings Street West	P.O. Box 5000	Brockville	ON	K6V 2A5	613-342-8772 x 3305		matt.locks@brockville.com
		Mr.	Stephen	Wilson	Supervisor of Engineering	1 Kings Street West	P.O. Box 5000	Brockville	ON	K6V 2A5	613-342-8772 x 3203		stephen.wilson@etk.on.ca
		Ms.	Stephanie	Murphy	Executive Assistant to the Mayor and City Manager	1 Kings Street West	P.O. Box 5000	Brockville	ON	K6V 2A5	613-342-8772 x 4449		stephanie.murphy@brockville.com
		Andrew	McGinnis	Chief Planning Officer	City of Brockville	1 Kings Street West	P.O. Box 5000	Brockville	ON	K6V 2A5	613-342-8772 x 4449		andrew.mcginnis@brockville.com
Conservation Authorities													
		Tom	Beaubin	Manager, Conservation Lands	Cataratas Region Conservation Authority	1847 Perth Road	P.O. Box 190	Cambridge	ON	N3H 1R0	613-546-4228 ext 340	613-547-6474	tom.beaubin@crca.on.ca
		Mr.	Tom	Beaubin	Manager, Conservation Lands	15740 County Rd 2	PO Box 88	Leeds/Grenville	ON	K0J 1S9	1-800-432-0228		tom.beaubin@crca.on.ca
		Mr.	Tom	Beaubin	Manager, Conservation Lands	15740 County Rd 2	PO Box 88	Leeds/Grenville	ON	K0J 1S9	1-800-432-0228		tom.beaubin@crca.on.ca
Local Elected Representatives													
		Mr.	Steve	Clark	MPP, Minister of Municipal Affairs and Housing	1 Broad Street	Last 305	Brockville	ON	K6V 6Z4	613-342-9522	613-342-2501	www.steveclark.on.ca
		Mr.	Ernie	Burrow	Mayor	6544 New Dublin Rd		Addison	ON	R0E 1A0	613-342-9500		mayor@etk.on.ca
Emergency Services													
		Chief	Jeff	Burgart	Fire Chief	Township of Elizabethtown-Kitley Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		jeff.burgart@etk.on.ca
		Deputy Fire Chief	Mike	Brownlee	Deputy Fire Chief	Township of Elizabethtown-Kitley Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		mike.brownlee@etk.on.ca
		Chief	Joe	Brownlee	Administrative Assistant	Township of Elizabethtown-Kitley Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		joe.brownlee@etk.on.ca
		Chief	Bob	Almigh	Fire Chief	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		rob.almigh@etk.on.ca
		Mr.	Simon	Hardy	Fire Department Coordinator	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-2400		simon.hardy@etk.on.ca
		Mr.	Simon	Hardy	Department Commander	Almigh Fire Department	44 Main St E	Levi	ON	R0E 1A0	613-498-240		



**Neighbourhood Mail Distribution Plan
by Delivery Installation**

C177360129

Mailed by: 8162546 STANTEC CONSULTING LTD.
Mailed on Behalf of: 8162546 STANTEC CONSULTING LTD.
Customer Reference: 165001328

Deposit Date: 2024/09/24 Deposit Location: HAMILTON LCD 5							
DI Postal Code	Secondary DI name	Reference #1 or #2	Title of Mail Piece	Delivery Start Date	Containers	Pieces	Version Spec. Trans. Req.
K6V4Y0	BROCKVILLE		Notice of PIC 1 - 4111-22-0	Upon receipt	1	2,126	No Yes

Highway 401 Planning Study in the Township of Elizabethtown-Kitley (GWP 4111-22-00)
 Indigenous Communities - Project Mailing List

Indigenous Communities											
Title	First Name	Last Name	Job Title	Company / Agency	Address	City/Town	Prov	Postal Code	Phone	Contact by Email	Email
Grand Chief	Abraham	Benedict	Chief	Mohawks of Akwesasne	CIA III 101 Tewesateni Rd., Kawehnoke	Akwesasne	ON	K6H SR7	613-575-2250		grand.chief@akwesasne.ca
	Corine	Francis	Consultation Coordinator	Mohawks of Akwesasne	CIA III 101 Tewesateni Rd., Kawehnoke	Akwesasne	ON	K6H SR7	613-575-2250		abraham.francis@akwesasne.ca
	Corine	Francis	Executive Assistant to the Grand Chief	Mohawks of Akwesasne	CIA III 101 Tewesateni Rd., Kawehnoke	Akwesasne	ON	K6H SR7	613-575-2250		corine.francis@akwesasne.ca
	Cactus	Sunday	ARRD Manager	Mohawks of Akwesasne	CIA III 101 Tewesateni Rd., Kawehnoke	Akwesasne	ON	K6H SR7	613-575-2250		cactus.sunday@akwesasne.ca
	Kayla	Sunday	Manager of Environment	Mohawks of Akwesasne	CIA III 101 Tewesateni Rd., Kawehnoke	Akwesasne	ON	K6H SR7	613-575-2250		kayla.sunday@akwesasne.ca
Sir/Madam	Consultation	Coordinator	Consultation Unit	Metis Nation of Ontario	Suite 1100 - 66 Slater Street	Ottawa	ON	K1P 5H1	613-798-1488		consultations@metisnation.org

APPENDIX B PIC 1 Materials





hwy401eastofbrockville.ca

Highway 401

East of Brockville

Planning, Preliminary Design, and
Class Environmental Assessment Study

GWP 4111-22-00



Welcome to

Online Public Information Centre 1

The purpose of this online PIC is to present and gather your feedback on:

- Project background and the process being followed
- Transportation needs and opportunities in the study area
- The Sharpe's Lane Bridge replacement and Butler Creek Culvert rehabilitation or replacement alternative strategies, and the future footprints for the Highway 401 corridor that will be evaluated as part of this project
- Preliminary existing study area conditions

This online PIC also aims to answer questions you may have about the project.

Other information related to this PIC that is available on the project website includes:

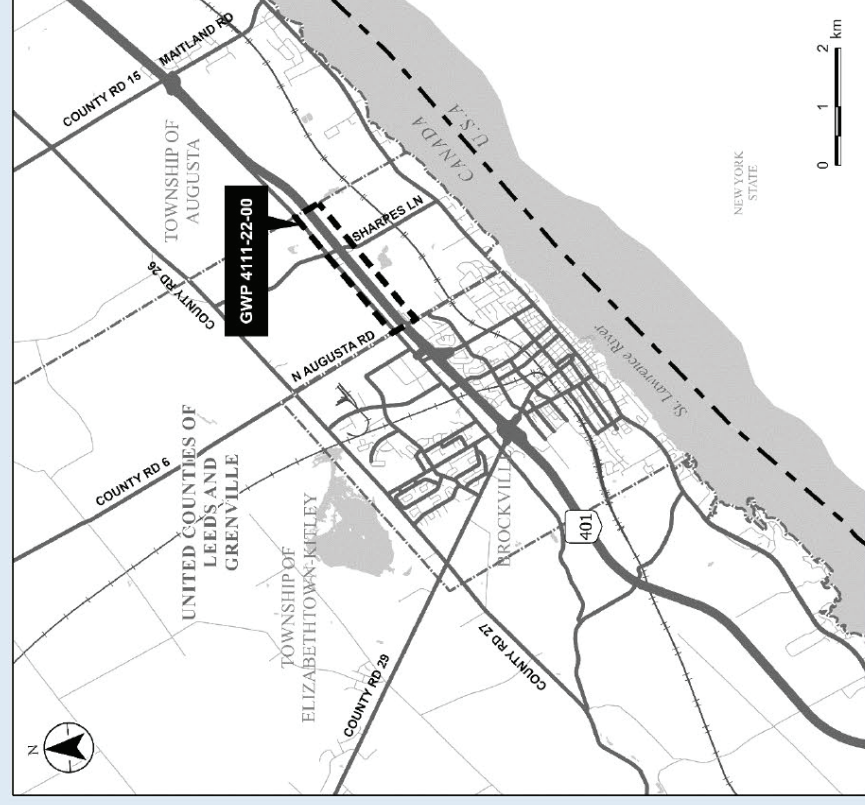
- A PDF copy of this presentation
- A PDF copy of the study area
- A PDF copy of the preliminary alternatives under consideration
- Online comment form

About The Project

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of Sharpe's Lane bridge and Butler Creek culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

Highway 401 is a controlled-access 400-series provincial highway that connects southwestern Ontario (Windsor) to Quebec, a total of 830 km. The highway, and the structures along it, were built in the 1950s and 1960s.

As part of this study, the project team will be reviewing existing conditions, developing and evaluating alternatives, identifying appropriate improvements, and developing environmental protection and mitigation measures. At the completion of the study, a recommended plan for the Highway 401 study area will be confirmed and designated (i.e. protected).



Class Environmental Assessment Process Group 'B' Projects

Environmental Assessment Process

This study is being carried out under the requirements of the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024), which is approved under the Ontario Environmental Assessment Act for provincial transportation projects of a defined scope and magnitude.

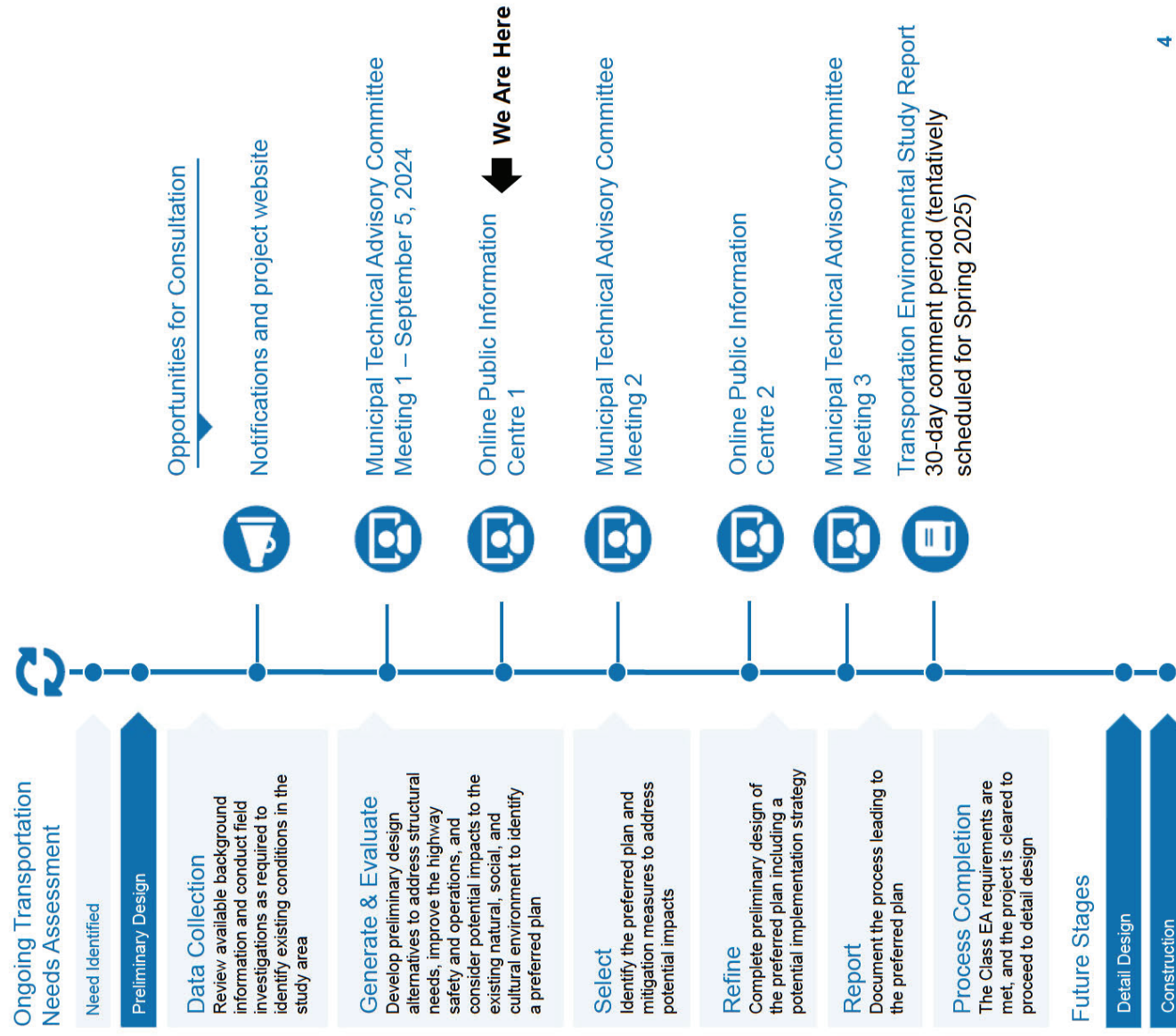
The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process, which includes major improvements to existing provincial transportation facilities.

In 2019, modernization of the MTO Class EA (2000) was initiated to optimize the Class EA process by removing redundancies and expediting planning to facilitate a more streamlined implementation process. Amendments to the Class EA were approved in December 2023 and February 2024, and Class EA was renamed the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024). As part of the amendments, the EA process is now complete at the end of Preliminary Design, following the 30-day comment period for the Notice of Completion.

As part of the new MTO Class EA (2024), Preliminary Design is the primary opportunity to provide feedback on the project. Opportunities for meaningful consultation in future stages of the project will be minimal.

At the end of the study, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day comment period.

[Please click here to access a copy of the MTO Class EA document](#)



Problem And Opportunities

The purpose of the study is to identify a recommended plan that addresses current and future transportation needs in the study areas as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network. This study will include reviewing existing conditions, developing and evaluating alternatives, identifying a preferred plan, and developing environmental protection/mitigation measures.

At the completion of the study, a Recommended Plan for Highway 401 will be confirmed and designated (i.e., protected). The Ministry will be registering a designation of a proposed highway plan based on the future needs determined by this Planning and Preliminary Design study. A designation is a mechanism for the Ministry to provide route/corridor protection as well as impose development control measures along the corridor and on adjoining owners. This type of designation is most often used where lands are still under private ownership, sometimes years in advance of intended property acquisition and construction.

This study has been initiated to address the following problem and opportunities:

Problem

- Sharpe's Lane Bridge and Butler Creek Culvert are nearing the end of their service life and will require rehabilitation and/or replacement in the near future
- The existing Highway 401 platform cannot accommodate the traffic staging required to rehabilitate or replace Sharpe's Lane Bridge and Butler Creek Culvert

Opportunities

- Develop appropriate rehabilitation or replacement strategies to maintain the safe operation of the highway corridor for the current and future planning horizons
- Identify the ultimate footprint for the Highway 401 corridor to address current and future transportation needs

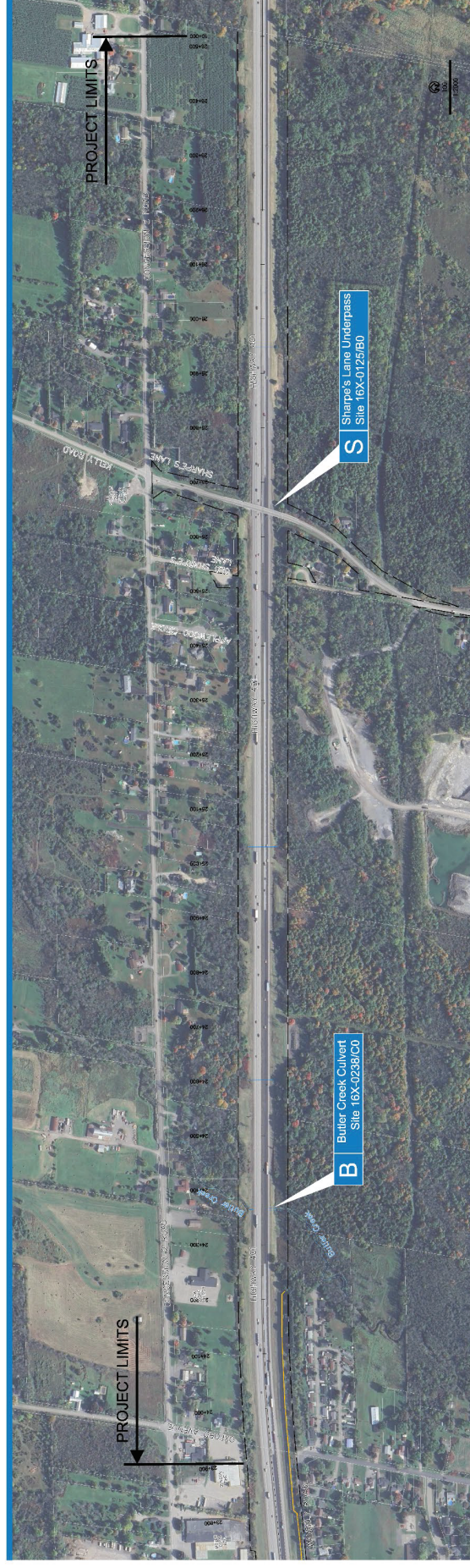
Alternatives To The Undertaking

The Class EA requires that 'reasonable alternatives' be considered to address identified problems. This involves two levels of analysis. The Alternatives to the Undertaking considers a broad range of alternatives that could address the project needs. Once the best alternative is selected, the Alternative Methods of Carrying out the Undertaking can be studied.

The Alternatives to the Undertaking identified for this study are listed below.

Alternatives to the Undertaking	Does it address the problems?
<p>Do Nothing Rehabilitate Sharpe's Lane Bridge and Butler Creek Culvert with no accommodation for the Highway 401 footprints for interim six and ultimate eight lanes.</p>	<p>✗ Does not address structural replacement needs and anticipated future growth needs. Do not carry forward.</p>
<p>Transportation Demand Management (TDM) TDM shifts demand on the highway network by shifting demands to the time periods outside of the critical congestion periods and shifts demand to alternative modes of transportation.</p>	<p>✗ Does not address structural replacement needs. Do not carry forward.</p>
<p>Improve Adjacent Road Systems Expansion of existing municipal and regional road networks.</p>	<p>✗ Does not address structural replacement needs. Do not carry forward.</p>
<p>Improved Provincial Transportation Facility Replace Sharpe's Lane Bridge and rehabilitate or replace Butler Creek Culvert to accommodate the Highway 401 footprints of interim six and ultimate eight lanes.</p>	<p>✓ Addresses structural and anticipated growth needs. Carry forward.</p>

Project Overview



Bridge Improvement

The Sharpe's Lane bridge is approaching the end of its service life and will need to be replaced. The bridge will need to accommodate the future Highway 401 footprint for interim six lanes and ultimate eight lanes.

> for more details, see:

[Bridge Improvement Alternatives](#)

S Sharpe's Lane bridge over Highway 401



Highway 401 Improvements

The study includes establishing the future Highway 401 footprint for the interim six lanes and ultimate eight lanes configurations.

> for more details, see:

[Highway 401 Alternatives](#)



Drainage Improvements

There is a structural culvert at Butler Creek which is approaching the end of its service life and will need to be rehabilitated or replaced. The culvert will need to accommodate the future Highway 401 footprint for interim six lanes and ultimate eight lanes. Existing structural and non-structural culverts are shown on the plan above.

> for more details, see:

[Drainage Improvements](#)

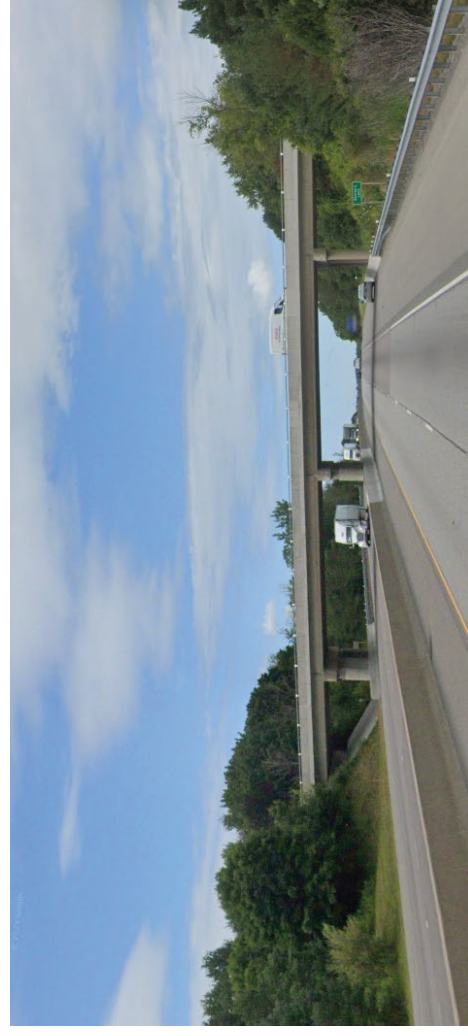
B Butler Creek Culvert

LEGEND

- Existing Highway 401 right-of-way
- Existing Structural Culvert
- Existing Non-Structural Culvert
- Existing Noise Barrier Wall

Please click here to review a high-resolution version of the [Project Overview plan](#)

Existing Structure Conditions



Sharpe's Lane Bridge – Site 16X-0125/B0

- Spans over 4 lanes of Highway 401
- Accommodates 2 lanes of traffic on Sharpe's Lane
- Constructed in 1967 and last rehabilitation in 2017
- Structure in overall good condition
- Does not accommodate the future Highway 401 footprint for an interim six lanes and ultimate eight lanes



Butler Creek Culvert – Site 16X-0238/C0

- Rigid frame structure
- Accommodates 4 lanes of Highway 401
- Constructed in 1959
- Structure in fair condition with several areas of deterioration
- Does not accommodate the future Highway 401 footprint for an interim six lanes and ultimate eight lanes

Preliminary Structure Alternatives

Sharpe's Lane Bridge

Bridge Improvement Alternatives



A range of alternatives for replacing this bridge have been developed. A preliminary screening has been carried out to identify the alternatives that should be carried forward for further consideration.

Carried-forward ✓
Screened-out ✗

Bridge closed with detour



Advantages

- Retains existing alignment of Sharpe's Lane
- Less property required compared to Alternatives 2 and 3
- Faster method of construction compared to staged construction with single lane
- Lower construction staging cost compared to keeping bridge open during construction

Disadvantages

- Impacts 2 hydro poles
- Introduces up to 8 km of travel for traffic to cross Highway 401 via detour route

Note: The final detour route will be confirmed in consultation with the affected municipalities.

Bridge open with single lane



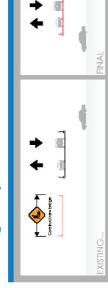
Advantages

- Retains existing alignment of Sharpe's Lane
- Maintains access across Highway 401 during construction
- Less property required compared to Alternatives 2 and 3

Disadvantages

- Impacts 2 hydro poles
- Minor traffic delays due to single lane of traffic across the bridge during construction
- Longer construction duration compared to closing the bridge
- Higher construction staging cost compared to closing the bridge

Bridge open with two lanes



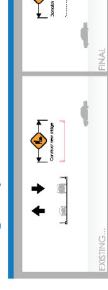
Advantages

- Maintains access across Highway 401 during construction with minimal impacts to traffic
- Similar construction duration as closing the bridge
- No hydro pole impacts

Disadvantages

- Requires alignment shift on Sharpe's Lane
- Requires more property compared to Alternatives 1 and 3
- Significantly higher cost compared to replacing on existing alignment

Bridge open with two lanes



Advantages

- Maintains access across Highway 401 during construction with minimal impacts to traffic
- Similar construction duration as closing the bridge
- Similar construction staging cost as closing the bridge

Disadvantages

- Requires alignment shift on Sharpe's Lane
- Results in undesirable horizontal curve on bridge
- Requires more property compared to Alternative 1
- Impacts 3 hydro poles
- Significantly higher cost compared to replacing on existing alignment

Do Nothing

Disadvantages

- Does not accommodate short-term or long-term structure needs

LEGEND

- New Roadway
- New Structure
- Property Required
- Limit of Existing MTO Right-of-way

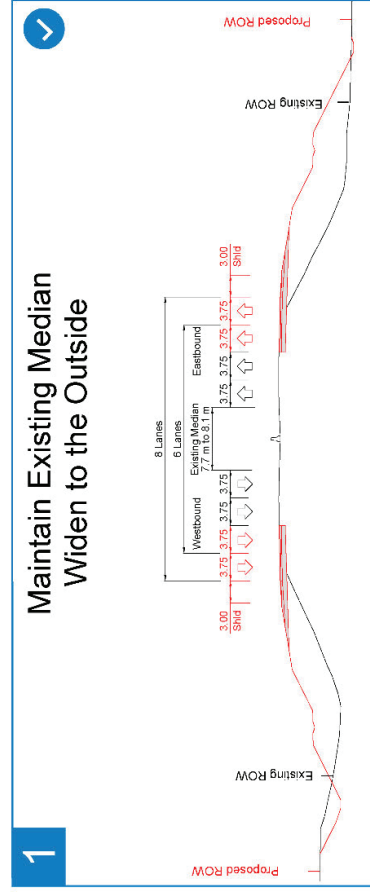
[Please click here to review a high-resolution version of Sharpe's Lane Alternatives](#)



Highway 401 Alternatives



Cross-Section Improvement Alternatives

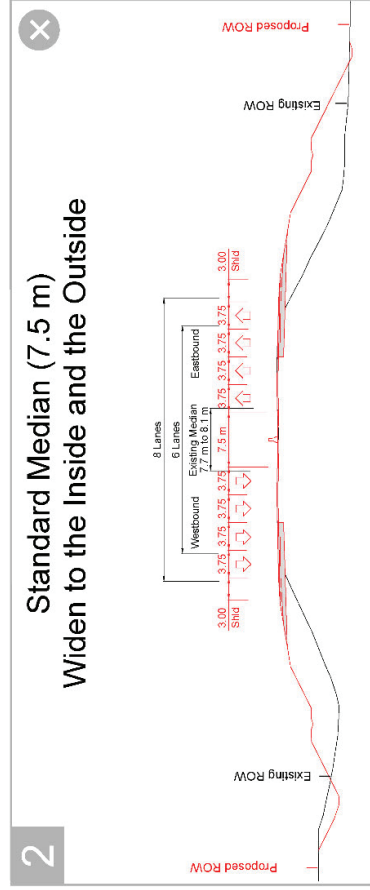


Advantages

- Retains the existing median with concrete barrier
- Minimizes impacts to traffic during construction
- Lower cost compared to Alternative 2

Disadvantages

- Requires grading outside of the existing highway footprint
- Requires more property and potential environmental impacts compared to Alternative 2



Advantages

- Provides standard median width with concrete barrier (7.5 m)
- Fewer property and potential environmental impacts compared to Alternative 1

Disadvantages

- Requires more complicated traffic staging compared to Alternative 1
- Requires reconstruction of the median including concrete barrier
- Higher cost compared to Alternative 1

LEGEND

Existing roadway
New roadway

[Please click here to review a high-resolution version of the Highway 401 Alternatives](#)



Drainage Improvements



B Butler Creek Culvert

Culvert Improvement Alternatives

Replace with new culvert:

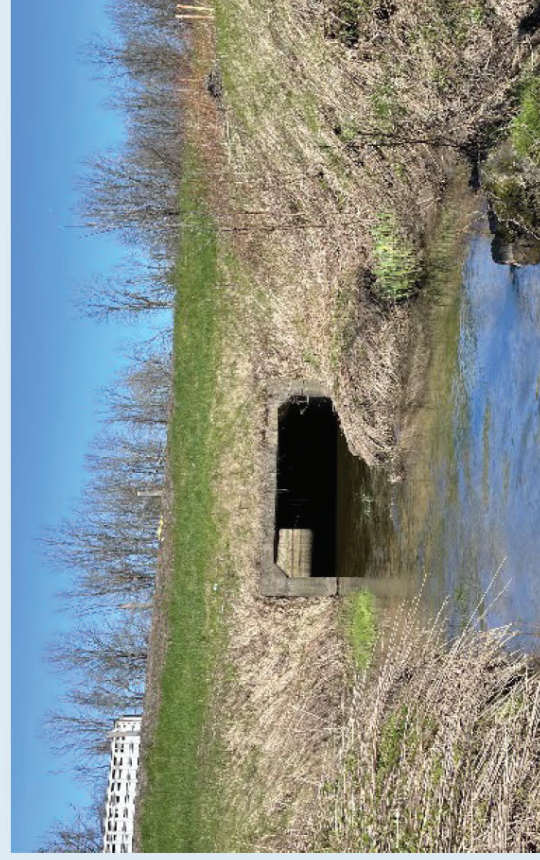
- + Long-term strategy
- + Accommodates interim (6-lane) Highway 401
- + Accommodates ultimate (8-lane) Highway 401
- Requires significant excavation

Culvert rehabilitation and retaining walls:

- Not a long-term strategy
- + Potential short-term strategy
- + Accommodates interim (6-lane) Highway 401
- Does not accommodate ultimate (8-lane) Highway 401
- Requires retaining walls

Culvert rehabilitation with extension:

- Not a long-term strategy
- + Potential short-term strategy
- + Accommodates interim (6-lane) Highway 401
- Does not accommodate ultimate (8-lane) Highway 401
- Future replacement of original culvert can be challenging



Butler Creek Culvert, Site 16X-0238/C0

11

Evaluation Process



Identify Criteria

Evaluation Criteria are established through:

- public input
- similar projects
- provincial guidelines
- existing conditions



Weigh Criteria

Each criterion is assigned a weight factor that best reflects its relative importance.



Evaluate Alternatives

All feasible alternatives are measured and given a score based on how well each is judged to satisfy the evaluation criteria



Rank Alternatives

The sum of the weighted scores provides a total score for each alternative. This is the basis for ranking the alternatives and identifying the Preferred Plan

How is the Recommended Plan Selected?

A detailed evaluation of alternatives will be carried out to identify an improvement plan that addresses structural and future transportation needs and provides safe operations, while minimizing the impacts to the natural, social and cultural environments. This is accomplished by identifying evaluation criteria along with their relative importance, and then ranking the overall scores of the design alternatives.

A second PIC will be scheduled following PIC 1 to present the evaluation of the alternatives and a Technically Preferred Plan for the Highway 401 Planning Study.

The Preferred Plan

The concluding step in the analysis and evaluation process is the selection of a Technically Preferred Plan.

This process includes:

- Reviewing the results of the analysis and evaluation based on specialist work and input received during the study
- Determining which criteria have the most influence on the outcome of the evaluation process
- Considering the sensitivity of the weightings
- Confirming the ranking of the alternatives
- Considering public/stakeholder response to the evaluation process

12

Preliminary Evaluation Criteria

Engineering

- **Geometrics & Safety**
- Design standards for provincial highways
- Potential for collisions on Highway 401
- Crossing road grades
- Pedestrian and cyclist accommodations

Constructability

- Construction techniques
- Traffic flow and operations, including local access and out-of-way travel

Utilities

- Impacts to utilities

Cost

- Consider total cost including utility relocations and property acquisition

Community

- **Property**
- Impacts to private property
- **Noise & Air Quality**
- Proximity to residences
- **Built & Cultural Heritage**
- Impacts to potential cultural heritage resources
- **Archaeology**
- Impacts to areas of archeological potential
- **Contamination**
- Potential to encounter contaminated soils/groundwater
- **Groundwater**
- Potential to impact groundwater recharge areas and highly vulnerable aquifers

Environment

- **Terrestrial Ecosystem**
- Impacts on wildlife habitat
- Impacts on significant trees and vegetation
- **Fish & Fish Habitat**
- Impacts to watercourses and fish and fish habitat
- **Species of Conservation Concern**
- Impacts to Species-at-Risk and habitat associated with Species-at-Risk
- **Designated Areas**
- Impacts to Significant Ecological Area



Bridge Improvements



Culvert/Drainage Improvements



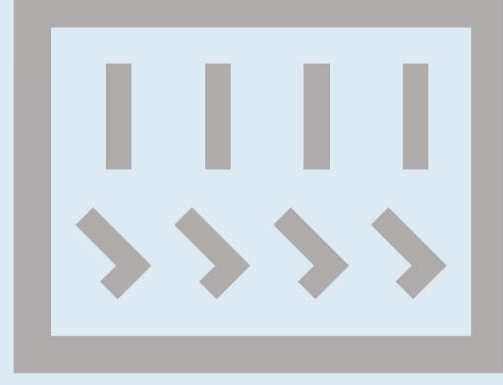
Highway Improvements

13

Next Steps

The following will be completed following this PIC:

- ✓ Review, consider and respond to the comments received
- ✓ Evaluate alternatives
- ✓ Identify the Technically Preferred Plan
- ✓ Identify the potential impacts and mitigation measures
- ✓ Host second Online PIC Spring 2025
- ✓ Prepare the Transportation Environmental Study Report (TESR)
- ✓ Issue Notice of Study Completion and File the TESR for a 30-day comment period (Spring/Summer 2025)



hwy401eastofbrockville.ca

14

Thank you for participating in this online PLC

Ways to provide your comments:



Complete the Online Comment Form



Email comments@hwy401eastofbrockville.ca



Contact by telephone:

Mr. Gregg Cooke, P.Eng.
Consultant Project Manager
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek ON L8J 0B4
Tel: 905-381-3227
comments@hwy401eastofbrockville.ca

Mr. David Brake
MTO Senior Project Manager
Ministry of Transportation
Asset Management – Capital Planning and Program Delivery
1355 John Counter Boulevard, Postal Bag 4000
Kingston ON K7L 5A3
Tel: 613-893-3031
Fax: 613-540-5106
comments@hwy401eastofbrockville.ca



Visit the Study website:
<http://www.hwy401eastofbrockville.ca>

We would appreciate receiving your comments by November 8, 2024



Freedom of Information and Protection of Privacy Act
Comments and information regarding this study are being collected to satisfy the requirements of the Ontario Environmental Assessment Act, and in accordance with the Freedom of Information and Privacy Act. With the exception of personal information, all comments will become part of the public record.

Highway 401 East of Brockville, Planning, Preliminary Design, and Class EA – Online Public Information Centre 1 Presentation Script

Slide 1 (Introduction)

Hello, and thank you for joining us for this online Public Information Centre for the Planning, Preliminary Design, and Class Environmental Assessment of Highway 401 East of Brockville.

This video presentation will provide you with an overview of this important Class Environmental Assessment (or Class EA) study.

Slide 2 (Purpose)

This is the first Public Information Centre (PIC) for this study. A copy of the information presented will be available for your review on the project website, www.hwy401eastofbrockville.ca.

The purpose of this online PIC is to present and gather your feedback on:

- Project background and the process being followed
- Transportation needs and opportunities in the study area
- The alternative bridge replacement and culvert rehabilitation or replacement strategies, and the future footprint for the Highway 401 corridor that will be evaluated as part of this project
- The existing study area conditions

This online PIC also aims to answer questions you may have about this study.

Additional resources are available for you to download on the project website so that you can take the time to review the information in more detail. Comments can also be submitted on the project website.

Slide 3 (About the Project)

The Ontario Ministry of Transportation has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of Sharpe's Lane Bridge and Butler Creek Culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 750 m east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 kilometers), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

Highway 401 is a controlled-access, 400-series provincial highway that connects southwestern Ontario (Windsor) to Quebec, a total of 830 km. The highway, and the structures along it, were built in the 1950s and 1960s.

As part of this study, the project team will be reviewing existing conditions, developing and evaluating alternatives, identifying appropriate improvements, and developing environmental protection and mitigation measures. At the completion of the study, a recommended plan for the Highway 401 study area will be confirmed and designated (in other words, protected for future improvements). This study builds off of previously completed studies on Highway 401 adjacent to this study area and aligns with the future highway footprint identified as part of those adjacent

studies. Additional information for those studies is available at: www.highway401brockville.ca and www.highway401prescottmaitland.ca

Slide 4 (EA Process)

This study is being carried out under the requirements of the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024), which is approved under the Ontario Environmental Assessment Act for provincial transportation projects of a defined scope and magnitude.

The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process, which includes projects that modify access or add capacity to existing provincial transportation facilities, and new service, maintenance, and/or operations facilities, such as new interchanges, intersections or roundabouts, and highway and freeway expansions.

In 2019, modernization of the MTO Class EA (2000) was initiated to optimize the Class EA process by removing redundancies and expediting planning to facilitate a more streamlined implementation process. Amendments to the Class EA were approved in December 2023 and February 2024, and Class EA was renamed the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024). As part of the amendments, the EA process is now complete at the end of Preliminary Design, following the 30-day comment period for the Notice of Completion.

As part of the new MTO Class EA (2024), Preliminary Design is the primary opportunity to provide feedback on the project. Opportunities for meaningful consultation in future stages of the project will be minimal.

At the end of the study, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day comment period.

The goal of projects or activities covered under this Class EA is to provide a safe and effective transportation system while avoiding or minimizing negative environmental effects. The Class EA process consists of two main stages: Planning and Preliminary Design. The Planning stage focuses on developing a plan for how the project should be done, and the Preliminary Design stage focuses on 'roughing out' a design.

There are a number of steps to be completed as part of the Planning and Preliminary Design phases of the project, which allow for structured decision making to occur as the study progresses. These steps include:

- Data Collection – where available background information is reviewed, and field investigations are conducted as required to identify existing conditions in the study area
- Generating and evaluating alternatives – at this stage, Preliminary Design alternatives are developed to address structural needs, improve the highway safety and operations, and consider potential impacts to the existing natural, social, and cultural environment to identify a preferred plan
- Selection of a preferred plan – this step includes the identification of a preferred plan and proposes mitigation measures to address potential impacts to the environment
- Refine the preferred plan – as part of this step, the Preliminary Design of the preferred plan, including an implementation strategy, is proposed

- Reporting – once the information from the steps completed above have been gathered, considered, and finalized, the entire process is documented within a Transportation Environmental Study Report, or TESR, which will be posted on the project website for a 30-day public comment period. Notification of TESR Completion will be posted on the project website, within the *Brockville Recorder and Times* and *Brockville This Week* newspapers, and delivered via mail/email to those who have expressed an interest in this project
- Process Completion – this is an internal process that MTO uses to help to ensure that the Class EA requirements have been met before subsequent stages including detail design and construction begin

Consultation with the public, agencies, municipalities, and Indigenous communities, as well as identifying environmental protection measures are elements of the Planning and Preliminary Design phase of the project. Opportunities for consultation during this study are shown on this slide and include Public Information Centres, Municipal Technical Advisory Committee Meetings, responses to public notices, comments via the project website, and comments on the Transportation Environmental Study Report during the 30-day comment period.

The Implementation stage of the project, which includes Detail Design and construction of the proposed improvements, is not part of the Class EA process and will be undertaken in the future. As noted previously, opportunities for meaningful consultation during this stage of the project will be minimal as the Implementation stage is not part of the Class EA process. The timing of the Implementation stage of the project is not yet known.

Slide 5 (Problems and Opportunities)

The purpose of the study is to identify and recommend a plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network. This study will include reviewing existing conditions, developing and evaluating alternatives, identifying a preferred plan, and developing environmental protection/mitigation measures.

At the completion of the study, a Recommended Plan for Highway 401 will be confirmed and designated (i.e., protected). The Ministry will be registering a designation of a proposed highway plan based on the future needs determined by this Planning and Preliminary Design study. A designation is a mechanism for the Ministry to provide route/corridor protection as well as impose development control measures along the corridor and on adjoining owners. This type of designation is most often used where lands are still under private ownership, sometimes years in advance of intended property acquisition and construction. The study has been initiated to address the following problems and opportunities:

Problems

- The bridge and structural culvert in the Study Area are nearing the end of their service life and will require rehabilitation and/or replacement in the near future.
- The existing Highway 401 platform cannot accommodate the traffic staging required to rehabilitate or replace the bridge and structural culvert

Opportunities

- Develop appropriate rehabilitation or replacement strategies to maintain the safe operation of the highway corridor for the current and future planning horizons
- Identify the ultimate footprint for the Highway 401 corridor to address current and future transportation needs

Slide 6 (Alternatives to the Undertaking)

The Class EA requires that 'reasonable alternatives' be considered to address identified problems. This involves two levels of analysis. The Alternatives to the Undertaking considers a broad range of alternatives that could address the project needs. Once the best alternative is selected, the Alternative Methods of Carrying out the Undertaking can be studied.

The Alternatives to the Undertaking identified for this study include:

- Do Nothing, which includes the rehabilitation of the existing bridge and culvert with no accommodation for the future Highway 401 footprints for interim six and ultimate eight lanes
- Transportation Demand Management, or TDM, changes the demand on the highway network by shifting demands to times outside of the critical congestion periods and towards alternative modes of transportation
- Improve Adjacent Road Systems includes the expansion of the existing municipal and regional road networks
- Improved Provincial Transportation Facility includes the replacement of the existing bridge and culvert to accommodate the Highway 401 footprints of interim six and ultimate eight lanes

Based on the findings of the assessment of the Alternatives to the Undertaking, the Improved Provincial Transportation Facility alternative addresses the structural and anticipated growth needs, and is carried forward.

Slide 7 (Project Overview)

As part of this study, bridge, drainage, and Highway 401 improvements are being considered. Sharpe's Lane Bridge is approaching the end of its service life and will need to be replaced. The bridge will need to accommodate the future Highway 401 footprints for the interim six and ultimate eight lanes.

In addition, the Butler Creek Culvert beneath Highway 401 is approaching the end of its service life and will need to be rehabilitated or replaced. The culvert will also need to accommodate the Highway 401 footprint for interim six and ultimate eight lanes.

Lastly, the study includes establishing the future Highway 401 footprints for the interim six lane and ultimate eight lane configurations.

Slide 8 (Existing Structure Conditions)

The existing condition of the two structures within the Study Area have been reviewed and assessed. A summary of the findings of this assessment is shown on this slide. Sharpe's Lane Bridge was constructed in 1967 and was last rehabilitated in 2017. The bridge is in overall good condition; however, it does not accommodate the future Highway 401 footprint for interim six

and ultimate eight lanes. Butler Creek Culvert was constructed in 1959 and is in fair condition with several areas of deterioration. The culvert also does not accommodate the future Highway 401 footprint for interim six and ultimate eight lanes.

Slide 9 (Preliminary Structure Alternatives)

As noted, this study includes the preliminary design for modifications to Sharpe's Lane Bridge.

A range of alternatives for replacing this bridge have been developed, including replacement of the bridge on its existing alignment, on a new alignment to the west, and on a new alignment to the east. A preliminary screening of these and the 'Do Nothing' alternative was carried out, and it was determined that the bridge replacement alternatives should be carried forward for further consideration. The 'Do Nothing' alternative was not carried forward as it does not accommodate the short-term or long-term structure needs.

Slide 10 (Highway 401 Alternatives)

Two alternatives for improvements to Highway 401 were developed. Alternative 1 includes maintaining the existing median and widening the highway to the outside, and Alternative 2 includes a standard median and widening the highway to both the inside and outside.

The preliminary screening determined that only Alternative 1 should be carried forward as it retains the existing median and concrete barrier, minimizes impacts to traffic during construction, and has a lower cost compared to Alternative 2. Alternative 2 was screened out as it requires more complicated traffic staging compared to Alternative 1, requires reconstruction of the median including concrete barrier, and has a higher cost compared to Alternative 1.

Slide 11 (Drainage Improvements)

Butler Creek Culvert, Site 16X-0238/C0, is the only structural culvert in the Study Area. Three improvement strategies are being considered for the existing culvert, including replacement with a new culvert, culvert rehabilitation and construction of retaining walls, and culvert rehabilitation with a culvert extension. Both rehabilitation options are not considered long-term strategies as they do not accommodate the ultimate 8-lane Highway 401 configuration; however, they will both accommodate the interim 6-lane Highway 401 configuration. Replacement of the existing culvert with a new culvert will accommodate both the interim 6-lane and ultimate 8-lane Highway 401 configuration but requires significant excavation. All of the improvement strategies will be carried forward for further consideration.

Slide 12 (Evaluation Process)

A detailed evaluation of alternatives will be carried out to identify an improvement plan that addresses structural and future transportation needs and provides safe operations, while minimizing the impacts to the natural, social, and cultural environments. This is accomplished by identifying evaluation criteria along with their relative importance, and then ranking the overall scores of the design alternatives.

A second PIC will be scheduled following PIC 1 in order to present the evaluation of the alternatives and a Technically Preferred Plan for this Highway 401 Planning Study.

The concluding step in the analysis and evaluation process is the selection of a Technically Preferred Plan. This process includes:

- Reviewing the results of the analysis and evaluation based on specialist work and input received during the study
- Determining which criteria have the most influence on the outcome of the evaluation process
- Considering the sensitivity of the weightings
- Confirming the ranking of the alternatives
- Considering public/stakeholder response to the evaluation process

Slide 13 (Preliminary Evaluation Criteria)

The evaluation process includes identifying evaluation criteria through the input received during this study, the project team's experience in projects of this nature, provincial guidelines, and existing study area conditions.

Preliminary criteria have been developed and are presented on this slide for your review. These include:

- Engineering criteria, including Geometrics and Safety; Constructability; Utilities; and Cost considerations
- Community related criteria, including Property; Noise & Air Quality; Built & Cultural Heritage; Archaeology; and Contamination considerations
- Environmental criteria, including Terrestrial Ecosystem; Fish & Fish Habitat; Species of Conservation Concern; and Significant Groundwater Recharge Areas

Following this PIC, the evaluation criteria will be confirmed and assigned a weight factor. A comparative analysis of transportation benefits and environmental effects will be completed for each alternative.

The Technically Preferred Plan is selected as the aggregate of Preliminary Design alternatives that achieve the best overall balance of transportation engineering, individual environmental factor impacts, and overall environmental impact, taking into consideration the net environmental effects by applying conceptual mitigation measures.

Comments received from the public, Indigenous Communities, agencies, and other stakeholders will also be considered as part of the evaluation process.

Slide 14 (Next Steps)

The Project Team will complete the following after this Public Information Centre:

- Review, consider, and respond to the comments received
- Evaluate alternatives
- Identify the Technically Preferred Plan
- Identify the potential impacts and mitigation measures
- Host the second Online Public Information Centre for the project, which is tentatively scheduled for Spring 2025
- Prepare the Transportation Environmental Study Report for the project
- Issue a Notice of Study Completion and file the Transportation Environmental Study Report for a 30-day comment period, which is tentatively scheduled for Spring/Summer 2025



**Online Public Information Centre (PIC) 1
Comment Sheet**

Slide 15 (Thank you)

On behalf of the Ontario Ministry of Transportation, we would like to thank you for your interest in this study, and for taking the time to participate in this online Public Information Centre. Your input is very important to us, and there are several ways to provide your comments.

We encourage you to review all PIC materials available on the project website, as well as to participate in the survey offered at the end of this presentation.

If you have any comments or questions, please do not hesitate to contact the project team via email at: comments@hwy401eastofbrockville.ca, or by contacting one of the project team members listed on this slide by telephone.

We would appreciate receiving any comments or questions you may have by November 8th, 2024. Thank you again for taking the time to participate in this online PIC.

Your feedback is important to us. **Please submit your comments by November 8, 2024.**
With the exception of personal information, all comments will become part of the public record.

Comments may be submitted by mail, fax or email to:

Mr. Gregg Cooke, P.Eng. Consultant Project Manager Stantec Consulting Ltd. 200-835 Paramount Drive, Stoney Creek ON L8J 0B4 Tel: (905) 381-3227 Fax: (905) 385-3534 Email: comments@hwy401eastofbrockville.ca	Mr. David Brake Senior Project Manager Ministry of Transportation Capital Planning and Program Development 1355 John Counter Boulevard, Postal Bag 4000 Kingston ON K7L 5A3 Tel: 613-893-3031 Email: comments@hwy401eastofbrockville.ca
---	---

If you have any accessibility requirements to participate in this project, please contact one of the Project Team members listed above.

Comments submitted by:

Name: _____

Address (including Postal Code): _____

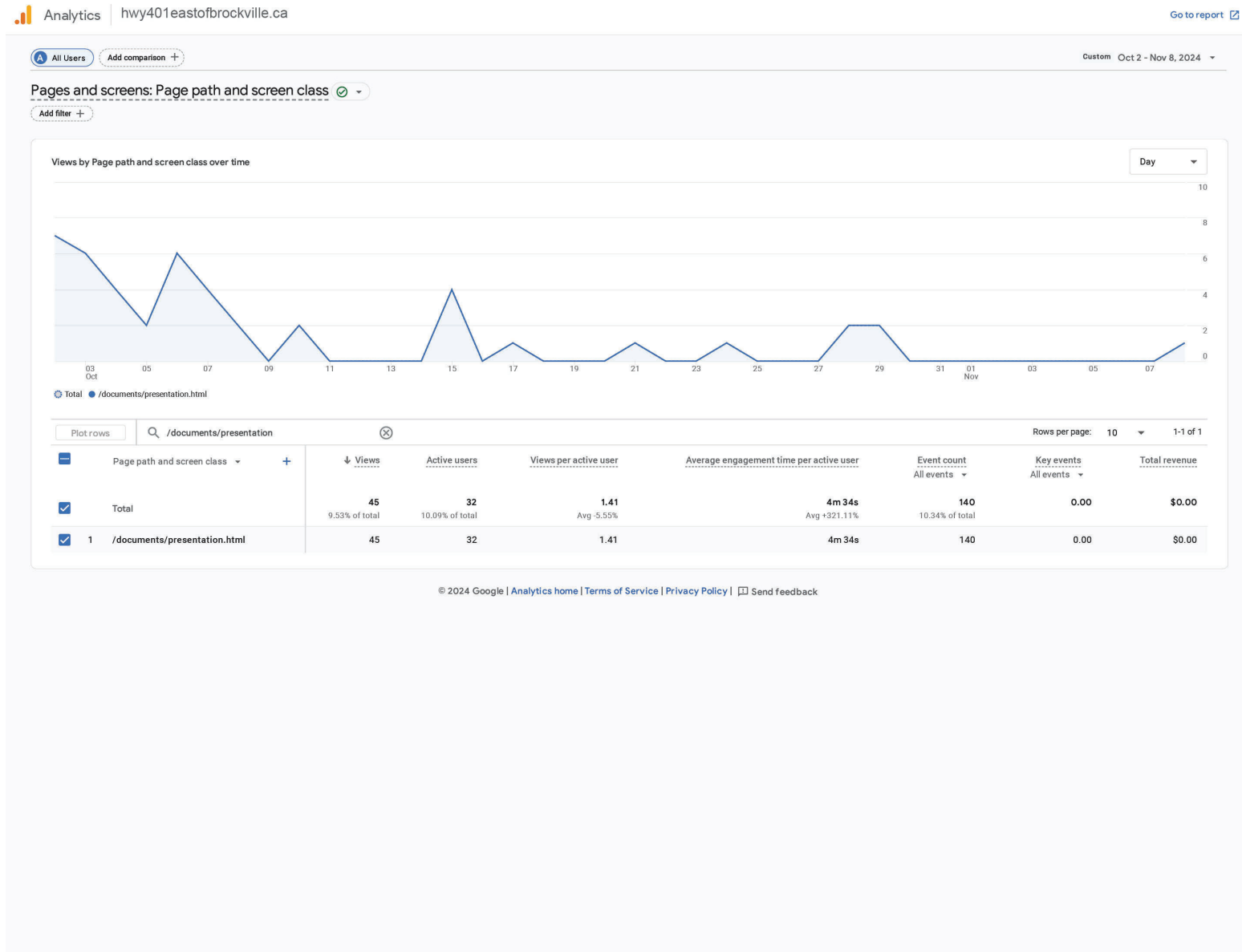
Telephone Number: _____

Email: _____

Please provide your comments in the space provided below:

**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 1**

0 APPENDIX D
Comments Received
November 27, 2024



**APPENDIX D
Comments Received**



Agency Comments

From: [Adams, Tracy \(MNR\)](#)
To: [Vanhell, Shane](#); [Worth, Adam \(MNR\)](#)
Cc: [Southern Region Planning Inbox \(MNR\)](#)
Subject: RE: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00)
Date: Thursday, September 26, 2024 12:22:36 PM
Attachments: [Notice of Public Information Centre 1 \(GWP 4111-22-00\).pdf](#)

You don't often get email from tracy.adams2@ontario.ca. [Learn why this is important](#)

Good afternoon Shane,

This area falls within the Kemptville Kingston district. I've copied Adam Worth, DM for that district for awareness .

Thanks,
Tracy

From: Vanhell, Shane <Shane.Vanhell@stantec.com>
Sent: Wednesday, September 25, 2024 10:38 AM
To: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>; Leclerc, Erika (MCM) <erika.leclerc@ontario.ca>; Barboza, Karla (She/Her) (MCM) <Karla.Barboza@ontario.ca>; Smythe, Liam (He/Him) (MCM) <Liam.Smythe@ontario.ca>; Kelly, Tate <Tate.Kelly@infrastructureontario.ca>; noticereview@infrastructureontario.ca; Adams, Tracy (MNR) <Tracy.Adams2@ontario.ca>; Warren, Catherine (She/Her) (MNR) <Catherine.Warren@ontario.ca>; Orpana, Jon (MECP) <Jon.Orpana@ontario.ca>; ldrynan@ektp.ca; deputyclerk <deputyclerk@ektp.ca>; planning@ektp.ca; mczarny@ektp.ca; jhutton@ektp.ca; scassidy@ektp.ca; ray.callery@uclg.on.ca; rick.kester@uclg.on.ca; shirley.white@uclg.on.ca; cherie.mills@uclg.on.ca; bill.guy@uclg.on.ca; chris.morrison@uclg.on.ca; sallen@brockville.com; mlocke@brockville.com; pwood@brockville.com; sdunbar@brockville.com; amcginnis@brockville.com; tbeaubiah@crca.ca; michael.barrett@parl.gc.ca; info@a2acollaborative.org; firechief <firechief@ektp.ca>; Deputy Chief <deputychief@ektp.ca>; Holly Dunster <fireadmin@ektp.ca>; Rob Bowman <rbowman@augusta.ca>; Ashleigh Trickey <atrickey@augusta.ca>; Hardy, Simon (OPP) <Simon.Hardy@opp.ca>; fireadmin@townshipleeds.on.ca; Carss, Jeff (MOH External) <jeff.carss@uclg.on.ca>; Ferguson, Ron <ron.ferguson@ucdsb.on.ca>; director@cdsbeo.on.ca; Marc Bertrand <bertrma@ecolecatholique.ca>; transportation@steo.ca; pjrobertson@brockvillechamber.com; ann.weir@uclg.on.ca; karen.mcdowell@ontruck.org; canada.marketing@coachcanada.com; cathy.lennon@ofa.on.ca; tyler.brooks@ofa.on.ca; brutusdanis@aol.com; annber@xplornet.com; gchs@truespeed.ca; terry.white@lafarge.com; greg.odonnell@leasecocommercial.com; kerianne012@gmail.com; ldickson@cogeco.ca; john.maskell@group-works.com; Burns.j@sympatico.ca; Gvkossivas@rogers.com; Stockwellr@rogers.com; len.meilleur@gmail.com; icampbell8@hotmail.com
Cc: comments@hwy401eastofbrockville.ca
Subject: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00)

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Good morning,

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and to identify the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of this email is to advise you of the first online Public Information Centre (PIC) planned for this study. As indicated within the attached notice, the online PIC will be available for your review on the study website at www.hwy401eastofbrockville.ca, starting on October 2, 2024. A recorded presentation will be available as part of the online PIC. The project team is requesting that comments be submitted by November 8, 2024.

The information presented at this online PIC will provide you with an opportunity to review and comment on the existing study area conditions, identified challenges and opportunities, preliminary alternatives, and proposed evaluation criteria.

You are encouraged to visit the project website, www.hwy401eastofbrockville.ca, to obtain current project information and to submit comments to the project team.

If you are unable to view the online PIC and would like further information regarding the study, please contact either the undersigned or one of the project team members named in the enclosed information.

Kind regards,

Shane Vanhell B. Sc.
Environmental Planner

Shane.Vanhell@stantec.com

Stantec
200-835 Paramount Drive
Stoney Creek ON L8J 0B4



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Please consider the environment before printing this email.

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Attention: Ce courriel provient de l'extérieur de Stantec. Veuillez prendre des précautions supplémentaires.

Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.

From:
To:
Subject:

Date:

From: annber@xplornet.com <annber@xplornet.com>

Sent: Tuesday, October 1, 2024 9:31 AM

To: Gazibara, Nevena <Nevena.Gazibara@stantec.com>

Subject: RE: Highway 401 Planning Study from Mallorytown to Brockville, GWP 4011-22-00-Municipal Technical Advisory Committee Meeting

Good Morning Nevena:

Thank you for the information. Due to the area you are working with, at this point in time, it doesn't concern the Grenville Snowmobile Association as our trails don't go west past Maitland.

Regards
Bernie Davy

From: Gazibara, Nevena <Nevena.Gazibara@stantec.com>

Sent: Monday, September 30, 2024 12:16 PM

To: comments@hwy401mallorytowntobrockville.ca

Cc: Cooke, Gregg <gregg.cooke@stantec.com>; Belliveau, Tim <tim.belliveau@stantec.com>; Brake, David (MTO) <dave.brake@ontario.ca>; Arsenault, Maddison (She/Her) (MTO) <Maddison.Arsenault@ontario.ca>; Vanhell, Shane <Shane.Vanhell@stantec.com>

Subject: RE: Highway 401 Planning Study from Mallorytown to Brockville, GWP 4011-22-00-Municipal Technical Advisory Committee Meeting

Good afternoon all,

The project team is following up from our previous email (sent on September 13, 2024) regarding the Municipal Technical Advisory Committee Meeting (MTAC) for the Planning, Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of bridges and culverts, interchange modifications, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 2.8 km west of the Mallorytown Road Interchange to 2.1 km west of the Stewart Boulevard interchange (approximately 21.6 km), in the Townships of Elizabethtown-Kitley and Front of Yonge, in the Counties of Leeds and Grenville.

The purpose of this email is to invite you to participate in a Municipal Technical Advisory Committee (MTAC) for the above-mentioned project. The purpose of the MTAC meetings is to provide project updates to key municipal staff members, obtain input on the study, design alternatives and the evaluation and selection of the preferred plan.

The purpose of the first MTAC meeting will be to introduce the project, seek your input on the existing conditions within the study area and to identify any issues or concerns that your organization may have.

Please provide a response noting your interest in joining the Municipal Technical Advisory Committee and if you would like the invitation extended to other staff at your organization. The project team will then follow-up with meeting information via email. Please also respond with your availability for the following dates and times for the first virtual MTAC meeting:

- **October 10th, 2024, at 10:00 AM**
- **October 16th, 2024, at 10:00 AM**
- **October 16th, 2024, at 2:00 PM**

Once we have confirmed a date and time for the first virtual MTAC meeting, we will send out a meeting invitation (with a Microsoft Teams link), an agenda and presentation materials.

You are encouraged to visit the project website, hwy401mallorytowntobrockville.ca, to obtain additional study information and/or to submit comments to the study team. If you have any questions regarding the study, please contact the undersigned.

Kind regards,

Nevena Gazibara B.Sc., MREM, ENV SP
Senior Environmental Planner
Environmental Team Lead - Transportation
nevena.gazibara@stantec.com

Pronouns: she, her, hers

Stantec
200-835 Paramount Drive
Stoney Creek ON L8J 0B4



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From: Gazibara, Nevena
Sent: Friday, September 13, 2024 4:28 PM
To: comments@hwy401mallorytowntobrockville.ca
Cc: Cooke, Gregg <gregg.cooke@stantec.com>; Belliveau, Tim <tim.belliveau@stantec.com>; Brake, David (MTO) <Dave.Brake@ontario.ca>; Arsenault, Maddison (She/Her) (MTO) <Maddison.Arsenault@ontario.ca>; Vanhell, Shane <Shane.Vanhell@stantec.com>
Subject: Highway 401 Planning Study from Mallorytown to Brockville, GWP 4011-22-00- Municipal Technical Advisory Committee Meeting

Good afternoon all,

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for

the replacement and rehabilitation of bridges and culverts, interchange modifications, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 2.8 km west of the Mallorytown Road Interchange to 2.1 km west of the Stewart Boulevard interchange (approximately 21.6 km), in the Townships of Elizabethtown-Kitley and Front of Yonge, in the Counties of Leeds and Grenville.

The purpose of this email is to invite you to participate in a Municipal Technical Advisory Committee (MTAC) for the above-mentioned project. The purpose of the MTAC meetings is to provide project updates to key municipal staff members, obtain input on the study, design alternatives and the evaluation and selection of the preferred plan.

The purpose of the first MTAC meeting will be to introduce the project, seek your input on the existing conditions within the study area and to identify any issues, concerns that your organization may have.

Please provide a response noting your interest in joining the Municipal Technical Advisory Committee and if you would like the invitation extended to other staff at your organization. The project team will then follow-up with meeting information via email. Please also respond with your availability for the following dates and times for the first virtual MTAC meeting:

- **October 3rd, 2024 at 2:00 PM**
- **October 10th, 2024 at 10:00 AM**
- **October 16th, 2024 at 10:00 AM**
- **October 16th, 2024 at 2:00 PM**

Once we have confirmed a date and time for the first virtual MTAC meeting, we will send out a meeting invitation (with a Microsoft Teams link), an agenda and presentation materials.

You are encouraged to visit the project website, hwy401mallorytowntobrockville.ca, to obtain additional study information and/or to submit comments to the study team. If you have any questions regarding the study, please contact the undersigned.

Nevena Gazibara B.Sc., MREM, ENV SP
Senior Environmental Planner
Environmental Team Lead - Transportation
nevena.gazibara@stantec.com

Pronouns: she, her, hers

Stantec
200-835 Paramount Drive
Stoney Creek ON L8J 0B4



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From:
To:
Subject:
Date:
Attachments:

From: Norman, Brandan (MECP) <Brandan.Norman@ontario.ca>
Sent: Tuesday, October 15, 2024 9:45 AM
To: Gazibara, Nevena <Nevena.Gazibara@stantec.com>
Cc: Cooke, Gregg <gregg.cooke@stantec.com>; Belliveau, Tim <tim.belliveau@stantec.com>; Brake, David (MTO) <dave.brake@ontario.ca>; Arsenault, Maddison (She/Her) (MTO) <Maddison.Arsenault@ontario.ca>; Dickson, Amanda (MTO) <Amanda.Dickson@Ontario.ca>; Vanhell, Shane <Shane.Vanhell@stantec.com>
Subject: RE: Highway 401 Planning Study from Mallorytown to Brockville (GWP 4011-22-00) Request for Virtual Meeting

Some people who received this message don't often get email from brandan.norman@ontario.ca. [Learn why this is important](#)

Hello Nevena,

My colleague forwarded your email to me as I'm the Management Biologist responsible for this part of the province.

I'm available to meet with you and your project team either October 28th at 10 AM or October 30th at 10 AM or 2 PM.

Thank you,
Brandan

Brandan Norman

Management Biologist | Species at Risk Branch
Ministry of the Environment, Conservation & Parks | Ontario Public Service
705-761-6850 | brandan.norman@ontario.ca

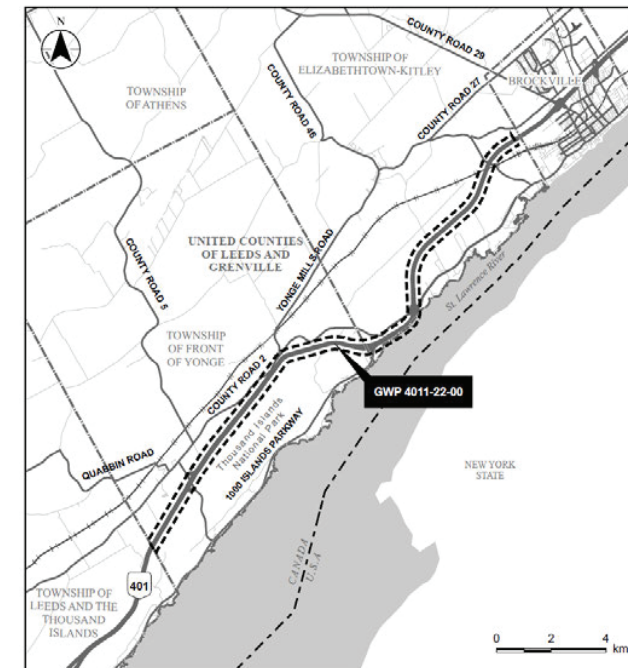


Taking pride in strengthening Ontario, its places and its people

From: Fowler, Devon (MECP) <Devon.Fowler@ontario.ca>
Sent: Thursday, October 10, 2024 5:00 PM
To: Norman, Brandan (MECP) <Brandan.Norman@ontario.ca>
Subject: FW: Highway 401 Planning Study from Mallorytown to Brockville (GWP 4011-22-00) Request for Virtual Meeting

Hi Brandan

Sorry for the delay in forwarding this to you – I kind of assumed it was in my area until I looked up the site map.



Thanks!

Devon Fowler | A/Management Biologist | Permissions Section | Species at Risk Branch
| Ministry of the Environment, Conservation & Parks |

From: Gazibara, Nevena <Nevena.Gazibara@stantec.com>
Sent: Tuesday, October 8, 2024 2:40 PM
To: Fowler, Devon (MECP) <Devon.Fowler@ontario.ca>
Subject: FW: Highway 401 Planning Study from Mallorytown to Brockville (GWP 4011-22-00) Request for Virtual Meeting

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From: Gazibara, Nevena
Sent: Tuesday, October 8, 2024 2:38 PM
To: sheldon.lambert@pc.gc.ca; Charette, Monique (MECP) <monique.charette@ontario.ca>; matthew.shakespeare@ontario.ca
Cc: Cooke, Gregg <gregg.cooke@stantec.com>; Belliveau, Tim <tim.belliveau@stantec.com>; Brake, David (MTO) <Dave.Brake@ontario.ca>; Arsenault, Maddison (She/Her) (MTO) <Maddison.Arsenault@ontario.ca>; Dickson, Amanda (MTO) <Amanda.Dickson@Ontario.ca>; Vanhell, Shane <Shane.Vanhell@stantec.com>
Subject: Highway 401 Planning Study from Mallorytown to Brockville (GWP 4011-22-00) Request for Virtual Meeting

Good afternoon,

The project team for the above-referenced project would like to schedule a virtual meeting with Parks Canada, the Ministry of Environment, Conservation and Parks, and the Ministry of Natural Resources to provide an overview of the study, the study process, and to discuss existing conditions in the study area.

Please provide a response noting your availability for the following dates and times for a virtual meeting with the project team:

- **October 18th, 2024, at 10:00 AM**
- **October 28th, 2024, at 10:00 AM**
- **October 29th, 2024, at 2:00 PM**
- **October 30th, 2024 at 10:00 AM or 2:00 PM**

Once we have confirmed a date and time for the meeting, we will send out a meeting invitation (with a Microsoft Teams link), an agenda and presentation materials. Please let us know if you would like anyone else from your organization invited to the meeting.

You are encouraged to visit the project website (hwy401mallorytowntobrockville.ca) to obtain additional study information and/or to submit comments to the study team. If you have any questions regarding the study, please contact the undersigned.

Kind regards,

Nevena Gazibara B.Sc., MREM, ENV SP
Senior Environmental Planner
Environmental Team Lead - Transportation
nevena.gazibara@stantec.com

Pronouns: she, her, hers

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From: [Barboza, Karla \(She/Her\) \(MCM\)](#)
To: [Vanhell, Shane](#)
Cc: [comments@hwy401eastofbrockville.ca](#); [Brake, David \(MTO\)](#); [Leclerc, Erika \(MCM\)](#)
Subject: RE: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00) [MCM File 0021593]
Date: Monday, October 21, 2024 12:08:08 PM
Attachments: [image001.png](#)
[Notice of Public Information Centre 1 \(GWP 4111-22-00\).pdf](#)

Hi Shane,

Thanks for sending this Notice of Public Information Centre 1 for the above referenced project to the Ministry of Citizenship and Multiculturalism (MCM).

We note that the Stage 1 archaeological assessment (under Project Information Form number P394-0122-2024) has yet to be submitted by your licensed archaeologist for MCM's review. We strongly encourage the assessment report be submitted to MCM before the issuance of the notice of completion.

Last June you advised that a Cultural Heritage Resource Assessment Report (CHRAR) would be undertaken. Could you please advise the status of the report?

Thanks in advance,
Karla

Karla Barboza, MCIP, RPP, CAHP (she/her)

Team Lead, Heritage | Heritage Operations Branch | Citizenship, Inclusion and Heritage Division

Ministry of Citizenship and Multiculturalism | Ontario Public Service
416-660-1027 | karla.barboza@ontario.ca



Taking pride in strengthening Ontario, its places and its people

From: Vanhell, Shane <Shane.Vanhell@stantec.com>
Sent: Wednesday, September 25, 2024 10:38 AM
To: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>; Leclerc, Erika (MCM) <erika.leclerc@ontario.ca>; Barboza, Karla (She/Her) (MCM) <Karla.Barboza@ontario.ca>; Smythe, Liam (He/Him) (MCM) <Liam.Smythe@ontario.ca>; Kelly, Tate <Tate.Kelly@infrastructureontario.ca>; noticereview@infrastructureontario.ca; Adams, Tracy (MNR) <Tracy.Adams2@ontario.ca>; Warren, Catherine (She/Her) (MNR) <Catherine.Warren@ontario.ca>; Orpana, Jon (MECP) <Jon.Orpana@ontario.ca>; ldrynan@ektwp.ca; deputyclerk <deputyclerk@ektwp.ca>; planning@ektwp.ca; mczarny@ektwp.ca; jhutton@ektwp.ca; scassidy@ektwp.ca; ray.callery@uclg.on.ca; rick.kester@uclg.on.ca; shirley.white@uclg.on.ca; cherie.mills@uclg.on.ca; bill.guy@uclg.on.ca; chris.morrison@uclg.on.ca; sallen@brockville.com; mlocke@brockville.com; pwood@brockville.com; sdunbar@brockville.com; amcginnis@brockville.com; tbeaubiah@crca.ca;

From: Vanhell, Shane <Shane.Vanhell@stantec.com>
Sent: Wednesday, September 25, 2024 10:38 AM
To: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>; Leclerc, Erika (MCM) <erika.leclerc@ontario.ca>; Barboza, Karla (She/Her) (MCM) <Karla.Barboza@ontario.ca>; Smythe, Liam (He/Him) (MCM) <Liam.Smythe@ontario.ca>; Kelly, Tate <Tate.Kelly@infrastructureontario.ca>; noticereview@infrastructureontario.ca; Adams, Tracy (MNR) <Tracy.Adams2@ontario.ca>; Warren, Catherine (She/Her) (MNR) <Catherine.Warren@ontario.ca>; Orpana, Jon (MECP) <Jon.Orpana@ontario.ca>; ldrynan@ektwp.ca; deputyclerk <deputyclerk@ektwp.ca>; planning@ektwp.ca; mczarny@ektwp.ca; jhutton@ektwp.ca; scassidy@ektwp.ca; ray.callery@uclg.on.ca; rick.kester@uclg.on.ca; shirley.white@uclg.on.ca; cherie.mills@uclg.on.ca; bill.guy@uclg.on.ca; chris.morrison@uclg.on.ca; sallen@brockville.com; mlocke@brockville.com; pwood@brockville.com; sdunbar@brockville.com; amcginnis@brockville.com; tbeaubiah@crca.ca; michael.barrett@parl.gc.ca; info@a2acollaborative.org; firechief <firechief@ektwp.ca>; Deputy Chief <deputychief@ektwp.ca>; Holly Dunster <fireadmin@ektwp.ca>; Rob Bowman <rbowman@augusta.ca>; Ashleigh Trickey <atrickey@augusta.ca>; Hardy, Simon (OPP) <Simon.Hardy@opp.ca>; fireadmin@townshipleeds.on.ca; Carss, Jeff (MOH External) <jeff.carss@uclg.on.ca>; Ferguson, Ron <ron.ferguson@ucdsb.on.ca>; director@cdsbeo.on.ca; Marc Bertrand <bertma@ecolecatholique.ca>; transportation@steo.ca; pjrobertson@brockvillechamber.com; ann.weir@uclg.on.ca; karen.mcdowell@ontruck.org; canada.marketing@coachcanada.com; cathy.lennon@ofa.on.ca; tyler.brooks@ofa.on.ca; brutusdanis@aol.com; annber@xplornet.com; gchs@truespeed.ca; terry.white@lafarge.com; greg.odonnell@leasecocommercial.com; kerianne012@gmail.com; ldickson@cogeco.ca; john.maskell@group-works.com; Burns.j@sympatico.ca; Gvkossivas@rogers.com; Stockwellr@rogers.com; len.meilleur@gmail.com; icampbell8@hotmail.com
Cc: comments@hwy401eastofbrockville.ca
Subject: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00)

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Good morning,

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and to identify the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of this email is to advise you of the first online Public Information Centre (PIC) planned for this study. As indicated within the attached notice, the online PIC will be available for your review on the study website at www.hwy401eastofbrockville.ca, starting on October 2, 2024. A recorded presentation will be available as part of the online PIC. The project team is requesting that comments be submitted by November 8, 2024.

The information presented at this online PIC will provide you with an opportunity to review and comment on the existing study area conditions, identified challenges and opportunities, preliminary alternatives,

and proposed evaluation criteria.

You are encouraged to visit the project website, www.hwy401eastofbrockville.ca, to obtain current project information and to submit comments to the project team.

If you are unable to view the online PIC and would like further information regarding the study, please contact either the undersigned or one of the project team members named in the enclosed information.

Kind regards,

Shane Vanhell B. Sc.
Environmental Planner

Shane.Vanhell@stantec.com

Stantec
200-835 Paramount Drive
Stoney Creek ON L8J 0B4



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michael.barrett@parl.gc.ca; info@a2acollaborative.org; firechief <firechief@ektwp.ca>; Deputy Chief <deputychief@ektwp.ca>; Holly Dunster <fireadmin@ektwp.ca>; Rob Bowman <rbowman@augusta.ca>; Ashleigh Trickey <atrickey@augusta.ca>; Hardy, Simon (OPP) <Simon.Hardy@opp.ca>; fireadmin@townshipleeds.on.ca; Carss, Jeff (MOH External) <jeff.carss@uclg.on.ca>; Ferguson, Ron <ron.ferguson@ucdsb.on.ca>; director@cdsbeo.on.ca; Marc Bertrand <bertrma@ecolecatholique.ca>; transportation@steo.ca; pjrobertson@brockvillechamber.com; ann.weir@uclg.on.ca; karen.mcdowell@ontruck.org; canada.marketing@coachcanada.com; cathy.lennon@ofa.on.ca; tyler.brooks@ofa.on.ca; brutusdanis@aol.com; annber@xplornet.com; gchs@truespeed.ca; terry.white@lafarge.com; greg.odonnell@leasecocommercial.com; kerianne012@gmail.com; ldickson@cogeco.ca; john.maskell@group-works.com; Burns.j@sympatico.ca; Gvkossivas@rogers.com; Stockwellr@rogers.com; len.meilleur@gmail.com; icampbell8@hotmail.com
Cc: comments@hwy401eastofbrockville.ca
Subject: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00)

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Good morning,

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and to identify the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of this email is to advise you of the first online Public Information Centre (PIC) planned for this study. As indicated within the attached notice, the online PIC will be available for your review on the study website at www.hwy401eastofbrockville.ca, starting on October 2, 2024. A recorded presentation will be available as part of the online PIC. The project team is requesting that comments be submitted by November 8, 2024.

The information presented at this online PIC will provide you with an opportunity to review and comment on the existing study area conditions, identified challenges and opportunities, preliminary alternatives, and proposed evaluation criteria.

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If you are unable to view the online PIC and would like further information regarding the study, please contact either the undersigned or one of the project team members named in the enclosed information.

Kind regards,

Shane Vanhell B. Sc.
Environmental Planner

Shane.Vanhell@stantec.com

Stantec
200-835 Paramount Drive
Stoney Creek ON L8J 0B4

From:
To:
Subject:

Date:
Attachments:

From: Barboza, Karla (She/Her) (MCM) <Karla.Barboza@ontario.ca>
Sent: Friday, October 25, 2024 10:19 AM
To: Gazibara, Nevena <Nevena.Gazibara@stantec.com>
Cc: comments@hwy401eastofbrockville.ca; Brake, David (MTO) <dave.brake@ontario.ca>; Leclerc, Erika (MCM) <erika.leclerc@ontario.ca>; Baczyk, Steve (MTO) <steve.baczyk@ontario.ca>; Cooke, Gregg <gregg.cooke@stantec.com>; Belliveau, Tim <tim.belliveau@stantec.com>; Vanhell, Shane <Shane.Vanhell@stantec.com>
Subject: RE: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00) [MCM File 0021593]

Thanks for the update, Nevena!
Karla

From: Gazibara, Nevena <Nevena.Gazibara@stantec.com>
Sent: Friday, October 25, 2024 10:05 AM
To: Barboza, Karla (She/Her) (MCM) <Karla.Barboza@ontario.ca>
Cc: comments@hwy401eastofbrockville.ca; Brake, David (MTO) <Dave.Brake@ontario.ca>; Leclerc, Erika (MCM) <erika.leclerc@ontario.ca>; Baczyk, Steve (MTO) <Steve.Baczyk@ontario.ca>; Cooke, Gregg <gregg.cooke@stantec.com>; Belliveau, Tim <tim.belliveau@stantec.com>; Vanhell, Shane <Shane.Vanhell@stantec.com>
Subject: RE: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00) [MCM File 0021593]

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Thank you for your email Karla.

The scope for the above-referenced study includes a Stage 1 Archaeological Assessment and a Cultural Heritage Resource Assessment Report. Our project team has completed their field investigations as part of these two studies and the draft reports are currently undergoing internal review, and then the reports will be submitted to MCM.

The reports will be completed and submitted to MCM prior to the Notice of Completion for the study.

Please let us know if you have any further questions at this time.

Thanks,
Nevena Gazibara B.Sc., MREM, ENV SP
Senior Environmental Planner
Environmental Team Lead - Transportation

nevena.gazibara@stantec.com

Pronouns: she, her, hers

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From: Barboza, Karla (She/Her) (MCM) <Karla.Barboza@ontario.ca>
Sent: Monday, October 21, 2024 12:04 PM
To: Vanhell, Shane <Shane.Vanhell@stantec.com>
Cc: comments@hwy401eastofbrockville.ca; Brake, David (MTO) <dave.brake@ontario.ca>; Leclerc, Erika (MCM) <erika.leclerc@ontario.ca>
Subject: RE: Notice of Public Information Centre 1 - Highway 401 East of Brockville (GWP 4111-22-00) [MCM File 0021593]

Hi Shane,

Thanks for sending this Notice of Public Information Centre 1 for the above referenced project to the Ministry of Citizenship and Multiculturalism (MCM).

We note that the Stage 1 archaeological assessment (under Project Information Form number P394-0122-2024) has yet to be submitted by your licensed archaeologist for MCM's review. We strongly encourage the assessment report be submitted to MCM before the issuance of the notice of completion.

Last June you advised that a Cultural Heritage Resource Assessment Report (CHRAR) would be undertaken. Could you please advise the status of the report?

Thanks in advance,
Karla

Karla Barboza, MCIP, RPP, CAHP (she/her)
Team Lead, Heritage | Heritage Operations Branch | Citizenship, Inclusion and Heritage Division
Ministry of Citizenship and Multiculturalism | Ontario Public Service
416-660-1027 | karla.barboza@ontario.ca



Taking pride in strengthening Ontario, its places and its people

Public Comments

From:
To:
Cc:
Subject:
Date:

-----Original Message-----

From:
Sent: Thursday, October 3, 2024 1:03 PM
To: comments@hwy401eastofbrockville.ca
Subject: Noise pollution

[You don't often get email from alisonastorey@icloud.com. Learn why this is important at <https://aka.ms/LearnAboutSenderIdentification>]

We live on Old Sharps Lane and are worried that the noise from the 6 lane highway will increase dramatically. Would it be possible to include a noise barrier in your plans?

Sent from my iPhone

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From:
To:
Cc:
Subject:
Date:

-----Original Message-----

From:
Sent: Thursday, October 3, 2024 11:19 AM
To: comments@hwy401eastofbrockville.ca
Subject: North Augusta Rd

[You don't often get email from ldickson@cogeco.ca. Learn why this is important at <https://aka.ms/LearnAboutSenderIdentification>]

Hi. We have been following this adventure since very early on as we . I looked on the map but I don't see any where how the issue of the off ramp on North Augusta rd has been addressed. Sometimes it is a very long wait to turn left to go over the overpass especially for the transports. Our original understanding was that the ramp was going to be moved to accommodate traffic turning left. Is this still going to happen. Thank you.
Tom and Linda Dickson

Sent from my iPad

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From:
To:
Cc:
Subject:
Date:

-----Original Message-----

From:
Sent: Sunday, October 6, 2024 12:42 PM
To: comments@hwy401eastofbrockville.ca
Subject: Add me to project mailing list

[You don't often get email from <https://aka.ms/LearnAboutSenderIdentification>] Learn why this is important at

Please add me to the project mailing list. I live at _____, Brockville just south of the 401. This project will affect me.
My email address is _____

Thank you.
Sent from my iPhone
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From:
To:
Subject:
Date: Monday, November 11, 2024 1:43:12 PM

-----Original Message-----

From:
Sent: Thursday, October 3, 2024 8:53 PM
To: comments@hwy401eastofbrockville.ca
Subject: Mailing list

You don't often get email from billmargfarmer@yahoo.ca. Learn why this is important
<<https://aka.ms/LearnAboutSenderIdentification>>

Please add our name to the project Mailing List.

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From: [Young, Rachel \(Waterloo\)](#)
To: [Young, Rachel \(Waterloo\)](#)
Subject: FW: Assessment GWP 4111-22-00
Date: Monday, November 11, 2024 1:43:27 PM

-----Original Message-----

From:
Sent: Thursday, October 3, 2024 1:58 PM
To: comments@hwy401eastofbrockville.ca
Subject: Assessment GWP 4111-22-00

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<<https://aka.ms/LearnAboutSenderIdentification>>

Good afternoon,

I received your notice in my mail today. Just FYI.... your map is wrong. You have mislabelled N. Augusta entirely... pls check google and try again. LOL

Thanks

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Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.

From:
To:
Subject: FW: Highway 401 Planning Study East of Brockville (GWP 4111-22-00)
Date: Tuesday, October 29, 2024 10:23:55 AM

From:

Sent: Tuesday, October 29, 2024 10:22 AM

To: Gazibara, Nevena <Nevena.Gazibara@stantec.com>

Subject: Re: Highway 401 Planning Study East of Brockville (GWP 4111-22-00)

You don't often get email from [. Learn why this is important](#)

Hello Nevena.

Can I get added to the email list for the GWP 4111-22-00 planning study.

I work For Lafarge Ready Mix in the Quality Control Division.

I cover the 401 Corridor from the Ont/QC border to the Kingston/Napanee area.

I live in Brockville and look forward to the 401 expansion and being a part of the rebuild.

Thank You.

Luke

On Tue, Oct 29, 2024 at 7:19 AM

> wrote:

----- Forwarded message -----

From: **Gazibara, Nevena** <Nevena.Gazibara@stantec.com>

Date: Mon, Oct 7, 2024 at 1:38 PM

Subject: Highway 401 Planning Study East of Brockville (GWP 4111-22-00)

To:

brake@ontario.ca, Arsenault, Maddison (She/Her) (MTO)
<Maddison.Arsenault@ontario.ca>, Dickson, Amanda (MTO)
<Amanda.Dickson@ontario.ca>, Cooke, Gregg <gregg.cooke@stantec.com>, Belliveau, Tim
<tim.belliveau@stantec.com>

Good afternoon

The project team for the above-referenced study is reaching out to you to gather feedback and input on

the Highway 401 Planning Study East of Brockville.

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The first online Public Information Centre (PIC) for this study is now available for review on the study website at: <http://www.hwy401eastofbrockville.ca>. The purpose of PIC 1 is to display and seek input on the existing conditions in the study area (i.e. natural, social, economic, and cultural), present the preliminary alternatives and proposed evaluation criteria, and answer questions about the study. The presentation includes preliminary bridge improvement alternatives at Sharpe's Lane Bridge.

We encourage you to view the PIC 1 presentation and let us know if you have any questions or comments regarding the study and in particular the alternatives at Sharpe's Lane Bridge and how they relate to operations at the Lafarge facility.

Kind regards,

Nevena Gazibara B.Sc., MREM, ENV SP
Senior Environmental Planner
Environmental Team Lead - Transportation
nevena.gazibara@stantec.com

Pronouns: she, her, hers

Stantec
200-835 Paramount Drive
Stoney Creek ON L8J 0B4



The content of this email is the confidential property of Stantec and should not be copied, modified, retransmitted, or used for any purpose except with Stantec's written authorization. If you are not the intended recipient, please delete all copies and notify us immediately.

--

Quality Control Technician ACI
Lafarge RMX
Mobile 1-613-791-6104

luke.langlois@lafarge.com

Microsoft Forms

**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 1**

0 APPENDIX E
Indigenous Engagement
November 27, 2024

ID	Start time	Completion time	Email	Name	Last modified time	Name:	Address (including Postal / Telephone Number Email)	Please provide your comment
1	10/3/24 17:26:20	10/3/24 17:29:44	anonymous				ON K6V 7J2 Brockville	I believe the project as proposed for the Sherpe's Lane Overpass and Butler Creek culvert make sense, if the objective is and it appears so, to increase the amount of lanes. This is prudent as the amount of traffic flowing through this two lane highway exceeds what it was designed for and is a risk to residents and commuters. My question is, what is the plan or where is the plan to be found that addresses the choke point between North Augusta to the east and Stewart Blvd to the west. Is that going to be completed at the same time, if so, great, if not, why?
2	10/4/24 21:53:39	10/4/24 21:59:08	anonymous				K6V 2A3 6L3	I believe that this project is very much needed, my only concern is the level of noise that residents along Waverly and Oxford Ave. will be forced to endure as well as the pollution from the expansion. The sound barrier that is in place now makes very little difference in the level of noise from the highway. We live between North Augusta Road and Sherpe's Lane. Glad to know that the ministry is tracking these structures and is trouble-shooting safety issues.
3			anonymous				K6V 2A3 6L3	

**APPENDIX E
Indigenous Engagement**



Ministry of Transportation

Regional Services & Relationships
East Operations Branch
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September 18, 2024
Page 2 of 2

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

September 18, 2024

Mohawks of Akwesasne
CIA III 101 Tewesateni Road, Kawehno:ke
Akwesasne ON K6H 5R7
grand.chief@akwesasne.ca

Attention: Grand Chief Abram Benedict

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

Dear Grand Chief Benedict:

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

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This study will include reviewing existing conditions, developing and evaluating alternatives, identifying appropriate improvements, and developing environmental protection and mitigation measures. A Recommended Plan will be confirmed and designated (protected) at the completion of the study.

Planned study activities include, but may not be limited to:

- Collecting and reviewing relevant background information and seeking input from the public, Indigenous Communities, and external ministries/agencies
- Carrying out field investigations to confirm environmental and engineering information
- Detailed natural science, fisheries, migratory bird, and noise studies
- A Stage 1 Archaeological Assessment

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A letter was sent to you in April 2024 to notify you of the commencement of the study, to provide you with information regarding environmental field investigations that are planned for this study, and to invite your input.

The purpose of this letter is to advise you that an online Public Information Centre (PIC) will be available for your review on the study website, www.hwy401eastofbrockville.ca, starting on **October 2, 2024**. A recorded presentation will be available as part of the online PIC.

This PIC provides an opportunity to review the preliminary alternatives, existing environmental conditions, and proposed evaluation process and criteria to be used to identify the Recommended Plan. The project website will also be regularly updated with current project information and can be used as an additional resource to submit comments to the project team.

If you have any questions or comments, or if you require further information regarding this project, please feel free to contact me, Franca Sacchetti, Director of East Operations at franca.sacchetti@ontario.ca, or Peter Copping, Indigenous Liaison Specialist at peter.a.copping@ontario.ca. The MTO will continue to engage with you as this project progresses.

Sincerely,

Franca Sacchetti
Director

c. D. Brake, P. Copping – Ministry of Transportation
G. Cooke, N. Gazibara – Stantec Consulting Ltd.
A. Francis, C. Francis, C. Sunday, K. Sunday – Mohawks of Akwesasne

Attachment: Notice of Public Information Centre 1

Ministry of Transportation

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September 18, 2024
Page 2 of 2

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

September 18, 2024

Mohawks of Akwesasne
CIA III 101 Tewesateni Road, Kawehno:ke
Akwesasne ON K6H 5R7
abraham.francis@akwesasne.ca

Attention: Abraham Francis, Consultation Coordinator

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

Dear Mr. Francis:

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

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Sincerely,

Franca Sacchetti
Director

c. D. Brake, P. Copping – Ministry of Transportation
G. Cooke, N. Gazibara – Stantec Consulting Ltd.

Attachment: Notice of Public Information Centre 1

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September 18, 2024
Page 2 of 2

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

September 18, 2024

Mohawks of Akwesasne
CIA III 101 Tewesateni Road, Kawehno:ke
Akwesasne ON K6H 5R7
corine.francis@akwesasne.ca

Attention: Corine Francis, Executive Assistant to the Grand Chief

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

Dear Ms. Francis:

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Sincerely,

Franca Sacchetti
Director

c. D. Brake, P. Copping – Ministry of Transportation
G. Cooke, N. Gazibara – Stantec Consulting Ltd.

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September 18, 2024
Page 2 of 2

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

September 18, 2024

Métis Nation of Ontario
Consultation Unit
Suite 1100 - 66 Slater Street
Ottawa ON K1P 5H1
consultations@metisnation.org

Attention: Consultation Coordinator

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

Dear Sir / Madam:

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Sincerely,

Franca Sacchetti
Director

c. D. Brake, P. Copping – Ministry of Transportation
G. Cooke, N. Gazibara – Stantec Consulting Ltd.

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September 18, 2024

Page 2 of 2

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

September 18, 2024

Mohawks of Akwesasne
CIA III 101 Tewesateni Road, Kawehno:ke
Akwesasne ON K6H 5R7
cactus.sunday@akwesasne.ca

Attention: Cactus Sunday, ARRO Manager**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

Dear Cactus Sunday:

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Sincerely,

Franca Sacchetti
Director

c. D. Brake, P. Copping – Ministry of Transportation
G. Cooke, N. Gazibara – Stantec Consulting Ltd.

Attachment: Notice of Public Information Centre 1

Ministry of Transportation

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September 18, 2024
Page 2 of 2

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

September 18, 2024

Mohawks of Akwesasne
CIA III 101 Tewesateni Road, Kawehno:ke
Akwesasne ON K6H 5R7
kayla.sunday@akwesasne.ca

Attention: Kayla Sunday, Manager of Environment

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 1**

Dear Ms. Sunday:

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

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Sincerely,

Franca Sacchetti
Director

c. D. Brake, P. Copping – Ministry of Transportation
G. Cooke, N. Gazibara – Stantec Consulting Ltd.

Attachment: Notice of Public Information Centre 1

**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 1**

0 APPENDIX F
Newspaper Articles
November 27, 2024

**APPENDIX F
Newspaper Articles**

A6 THURSDAY, OCTOBER 3, 2024 BROCKVILLE RECORDER AND TIMES



Conservative leader Pierre Poilievre arrives to Parliament Hill in Ottawa on Wednesday. He says focusing on legislation that promotes adoption would do 'greater good' to help pregnant women than restricting access to abortion, in a new letter. *SEAN RILPATRICK/THE CANADIAN PRESS*



Tory leader says he plans to promote adoption

Pierre Poilievre says adoption legislation would do 'greater good' in a new letter

STEPHANIE TAYLOR

OTTAWA Focusing on legislation that promotes adoption would do "greater good" to help pregnant women than restricting access to abortion, Conservative Leader Pierre Poilievre says in a new letter.

His office outlined the leader's position in correspondence sent to an anti-abortion supporter, who wrote the leader in early September calling on him to ban the procedure, suggesting Poilievre has changed his stance from being a "pro-lifer" over his 20-year career as a member of Parliament. Campaign Life Coalition, the anti-abortion organization that shared the exchange with the National Post, points to how Poilievre voted against gay marriage in 2005, but, by 2020, said he felt it had been a success.

"While my government will not

introduce or pass a law banning abortions, there are many other ways to support women who want to bring a child into the world. I do not believe that abortion should be the only option available to women faced with an unexpected pregnancy," Poilievre's letter from Sept. 11 reads.

"We can ease the challenges of putting a child up for adoption by supporting women through all nine months of pregnancy... I believe focusing on bills that promote adoption and help pregnant women through crises would do greater good."

Asked how a Poilievre government would make adoption easier, spokesman Sebastian Skamski did not provide details but instead slammed how the Liberals voted against a private member's bill proposed by Saskatchewan MP Rosemarie Falk, which sought to create a 15-week employment

insurance benefit for adoptive parents.

The Liberals ultimately introduced their own measure, which Falk herself told the House of Commons back in April provided "cause for cautious optimism."

Poilievre had supported her proposal and released a video where he reflected on his own adoption back in 1979, calling it "the greatest gift I've ever been given."

Since even before becoming leader, abortion has been among the thorny social issues Poilievre has had to navigate, which dogged previous leaders before him.

When Poilievre considered running for the Conservative leadership the first time in 2020, he said any future government of his would not reopen the issue, but would maintain free votes for MPs on matters of conscience.

Poilievre also confirmed during the party's 2022 leadership race that he considers himself to be "pro-choice."

Supreme Court decision overturning Roe v. Wade thrust the issue into the spotlight, which Poilievre responded to by saying that a future government of his would not introduce or pass any laws restricting abortion.

His wife, Anaida Poilievre, who is also campaigning for him, said during a French-language interview aired last December that the couple was "pro-choice."

The Liberals still point to the voting record and statements made by Poilievre's MPs, as the party has long been home to a well-mobilized social conservative wing, who believe the country must restrict access to abortion.

Prime Minister Justin Trudeau's Liberals have long attacked the Conservatives for having such views, including after a report last week that Saskatchewan MP Jeremy Patzer visited a U.S. church with anti-abortion views.

Poilievre's office was also quick to distance itself from statements made by Alberta backbench MP Arnold Viereen, who appeared on a Liberal MP's podcast to say he hopes for the day abortion is "unthinkable" and if given the chance again would vote down gay marriage.

The Liberals also attacked a private member's bill proposed by Saskatchewan MP Cathy Wagan-tall that was defeated last year and proposed to make it an aggravating

offence for someone to knowingly harm a pregnant woman — a measure that proponents of abortion access warned opened the door for legislating more protections to a fetus, though Conservatives argued the bill didn't even mention abortion.

In his September letter to the anti-abortion supporter, Poilievre said those who oppose the procedure have a home with the Conservatives.

"Pro-life Canadians are welcome in our party. The Conservative Party of Canada is a big tent with a strong tradition of free votes of conscience, and I intend to maintain this status quo," it read.

The letter also added that Conservatives oppose the Liberals' pledge to strip crisis pregnancy centres of their charitable status. Advocates of abortion access say many of them spread false and misleading information about the procedure.

Poilievre pledged to "stand up" to government attacks on "organizations that help pregnant women."

Skamski said as the country's possible next prime minister, "no laws or rules will be passed that restrict women's reproductive choices. Period."

"Common sense Conservatives support the value of adoption and supporting parents who provide a loving home to adopted children," he said. "Mr. Poilievre understands the power of adoption firsthand, having been born to a 16-year-old mother and adopted by two school teachers as a child."

He added that Canadians can expect a future Poilievre-led government to be one that "minds its own business," including when it comes to personal decisions about "their bodies."

*National Post
staylor@postmedia.com*

Notice of Public Information Centre 1

Preliminary Design and Class Environmental Assessment
Highway 401 Planning Study East of Brockville, GWP 4111-22-00

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of the study is to identify a Recommended Plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network.

This notice is to announce the first online Public Information Centre (PIC) for this study. The purpose of PIC 1 is to display and seek input on the existing conditions in the study area (i.e. natural, social, economic, and cultural), present the preliminary alternatives and proposed evaluation criteria, and answer questions about the study.

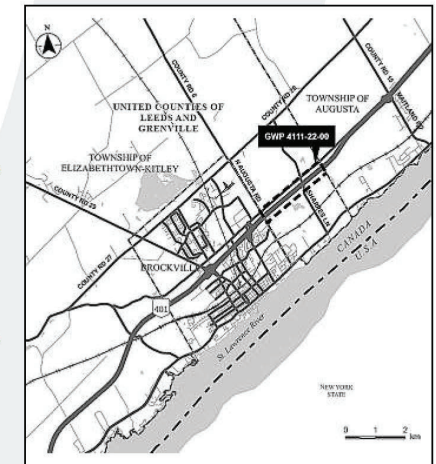
The online PIC will be available for review on the study website at www.hwy401eastofbrockville.ca starting October 2, 2024. A recorded presentation will be available as part of the online PIC until November 8, 2024.

THE PROCESS
This study is a "Group B" project under the Class Environmental Assessment (EA) for Provincial Transportation Facilities and Municipal Expressways (2024) and includes undertaking environmental and engineering field investigations and seeking input from indigenous communities, the public, stakeholders, municipalities and government agencies. Upon completion of preliminary design, a Transportation Environmental Study Report (TESR) will be prepared and made available for public comment. Notices will be published in local newspapers to advise the public of the TESR public comment period.

COMMENTS
You are encouraged to participate in the study and to provide comments in writing to the project team. If you wish to have your name added to the project mailing list or have questions about the study, please contact:

Gregg Cooke, P.Eng.
Consultant Project Manager
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek ON L8J 0B4
tel: 905-381-3227
call collect: 905-385-3234
e-mail: comments@hwy401eastofbrockville.ca

David Brake
Senior Project Manager
Ministry of Transportation Eastern Region
1355 John Counter Boulevard, Postal Bag 4000
Kingston ON, K7L 5A3
tel: 613-893-3031
fax: 613-540-5106
e-mail: comments@hwy401eastofbrockville.ca



You are encouraged to visit the project website, www.hwy401eastofbrockville.ca, where project information including study notices, background information, PIC displays, and the TESR will be made available as the study progresses. You can also submit comments to the project team on the project website. If you have any accessibility requirements to participate in this project, please contact one of the Project Team members listed above. Comments and information are being collected to assist the MTO in meeting the requirements of the Ontario Environmental Assessment Act. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.



Notice of Public Information Centre 1

Preliminary Design and Class Environmental Assessment Highway 401 Planning Study East of Brockville, GWP 4111-22-00

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville. The purpose of the study is to identify a Recommended Plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network.

This notice is to announce the first online Public Information Centre (PIC) for this study. The purpose of PIC 1 is to display and seek input on the existing conditions in the study area (i.e. natural, social, economic, and cultural), present the preliminary alternatives and proposed evaluation criteria, and answer questions about the study.

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THE PROCESS

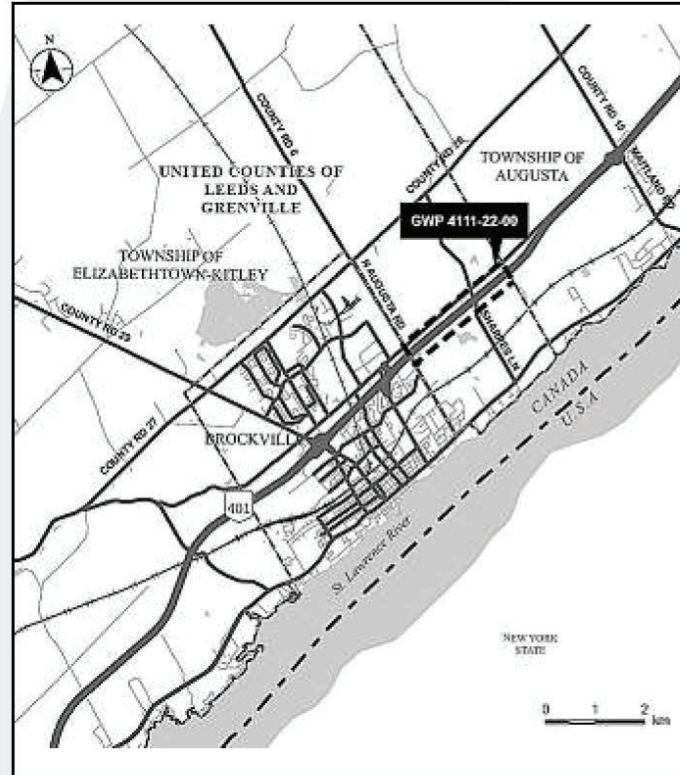
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COMMENTS

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ONLINE PUBLIC INFORMATION CENTRE 2



hwy401eastofbrockville.ca

Highway 401

East of Brockville

Planning, Preliminary Design, and
Class Environmental Assessment Study

GWP 4111-22-00



Welcome to

Online Public Information Centre 2

The purpose of this online PIC is to present and gather your feedback on the:

- Evaluation of Alternatives
- Technically Preferred Plan and associated rationale
- Preliminary design for the Technically Preferred Plan
- Potential environmental impacts and proposed mitigation measures

This online PIC also aims to answer questions you may have about the project.

Other information related to this PIC that is available on the project website includes:

- A PDF copy of this presentation
- A PDF copy of the technically preferred plan
- Online comment form

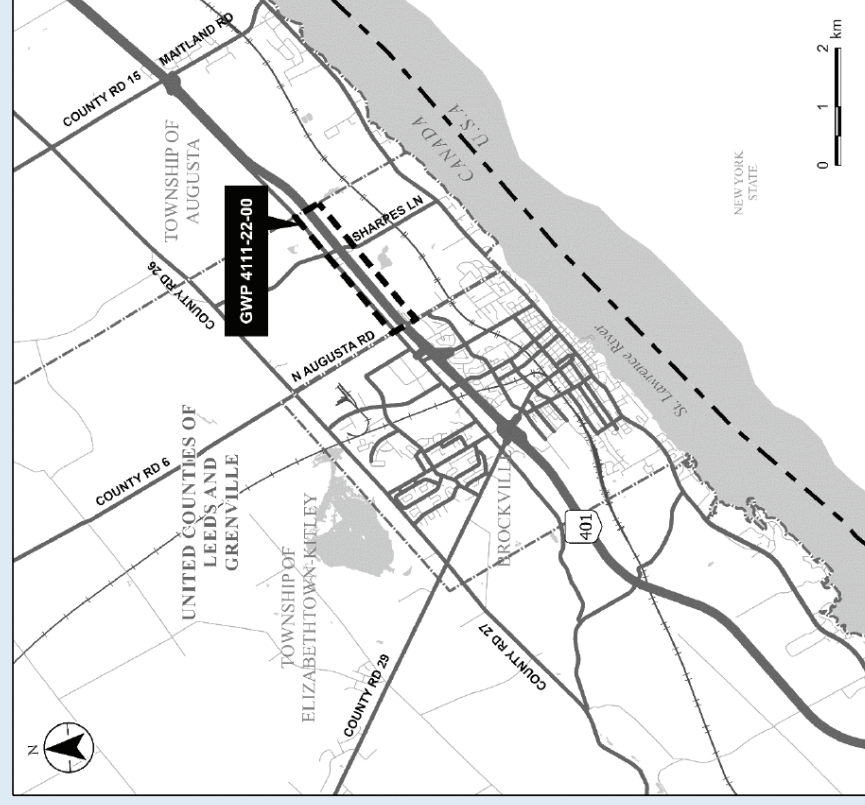


About The Project

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of Sharpe's Lane bridge and Butler Creek culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

Highway 401 is a controlled-access 400-series provincial highway that connects southwestern Ontario (Windsor) to Quebec, a total of 830 km. The highway, and the structures along it, were built in the 1950s and 1960s.

As part of this study, the project team reviewed existing conditions, developed and evaluated alternatives, identified appropriate improvements, and developed environmental protection and mitigation measures. At the completion of the study, a recommended plan for the Highway 401 study area will be confirmed and designated (i.e. protected).



Ontario



Class Environmental Assessment Process Group 'B' Projects

Environmental Assessment Process

This study is being carried out under the requirements of the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024), which is approved under the Ontario Environmental Assessment Act for provincial transportation projects of a defined scope and magnitude.

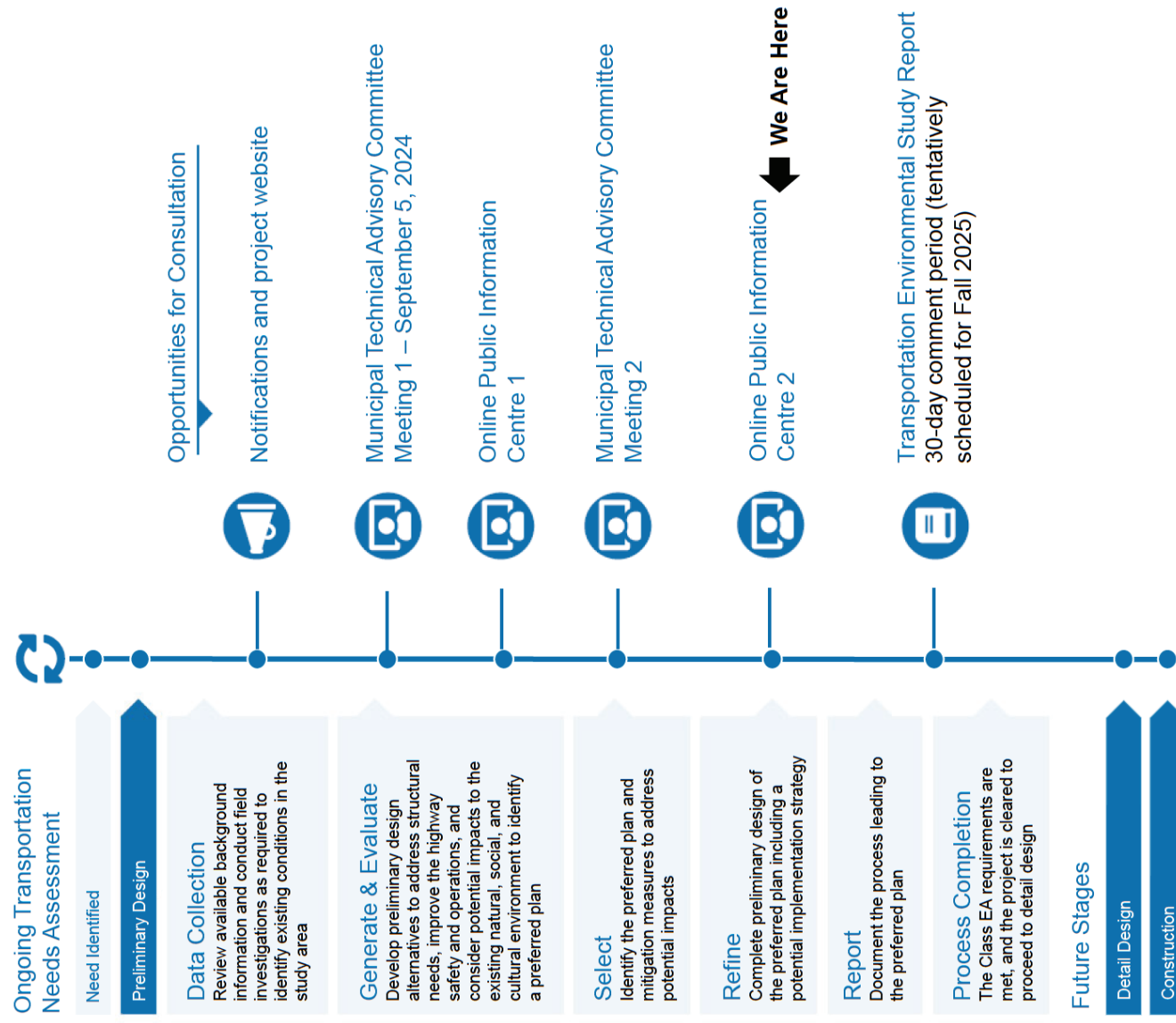
The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process, which includes major improvements to existing provincial transportation facilities.

In 2019, modernization of the MTO Class EA (2000) was initiated to optimize the Class EA process by removing redundancies and expediting planning to facilitate a more streamlined implementation process. Amendments to the Class EA were approved in December 2023 and February 2024, and Class EA was renamed the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024). As part of the amendments, the EA process is now complete at the end of Preliminary Design, following the 30-day comment period for the Notice of Completion.

As part of the new MTO Class EA (2024), Preliminary Design is the primary opportunity to provide feedback on the project. Opportunities for meaningful consultation in future stages of the project will be minimal.

At the end of the study, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day comment period.

Please [click here](#) to access a copy of the **MTO Class EA document**



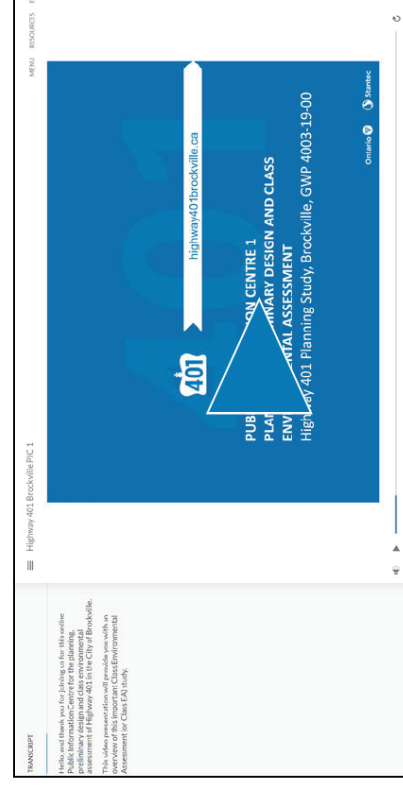
Online Public Information Centre 1 Summary

Online Public Information Centre 1 was held between October 2, 2024 - November 8, 2024, to present and gather feedback on:

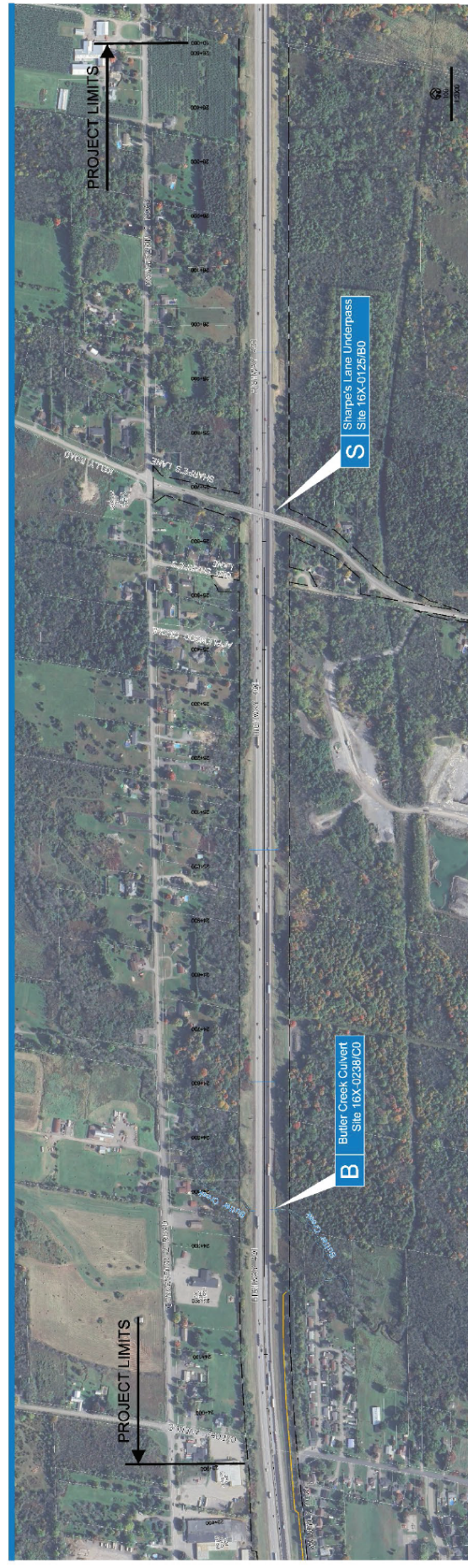
- Project background and the Class Environmental Assessment process
- Transportation needs and opportunities in the study area
- Preliminary structure alternatives for Sharpe's Lane Bridge
- Highway 401 widening alternatives
- Preliminary existing study area conditions

What did we hear?

- Potential impacts to property
- Potential impacts to safety and traffic
- Potential increases in noise
- Concerns with potential delays as a result of temporary closure of Sharpe's Lane Bridge during construction



Project Overview



Bridge Improvement

The Sharpe's Lane bridge is approaching the end of its service life and will need to be replaced. The bridge will need to accommodate the future Highway 401 footprint for interim six lanes and ultimate eight lanes.

> for more details, see:
Bridge Improvement Alternatives

S Sharpe's Lane bridge over Highway 401



Highway 401 Improvements

The study includes establishing the future Highway 401 footprint for the interim six lanes and ultimate eight lanes configurations.

> for more details, see:
Highway 401 Alternatives

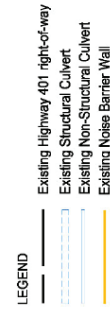


Drainage Improvements

There is a structural culvert at Butler Creek which is approaching the end of its service life and will need to be rehabilitated or replaced. The culvert will need to accommodate the future Highway 401 footprint for interim six lanes and ultimate eight lanes. Existing structural and non-structural culverts are shown on the plan above.

> for more details, see:
Drainage Improvements

B Butler Creek Culvert



[Please click here to review a high-resolution version of the Project Overview plan](#)

Problems And Opportunities

The purpose of the study is to identify a recommended plan that addresses current and future transportation needs in the study areas as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network. This study will include reviewing existing conditions, developing and evaluating alternatives, identifying a preferred plan, and developing environmental protection/mitigation measures.

At the completion of the study, a Recommended Plan for Highway 401 will be confirmed and designated (i.e., protected). The Ministry will be registering a designation of a proposed highway plan based on the future needs determined by this Planning and Preliminary Design study. A designation is a mechanism for the Ministry to provide route/corridor protection as well as impose development control measures along the corridor and on adjoining owners. This type of designation is most often used where lands are still under private ownership, sometimes years in advance of intended property acquisition and construction.

This study has been initiated to address the following problems and opportunities:

Problems

- Sharpe's Lane Bridge and Butler Creek Culvert are nearing the end of their service life and will require rehabilitation and/or replacement in the near future
- The existing Highway 401 platform cannot accommodate the traffic staging required to rehabilitate or replace Sharpe's Lane Bridge and Butler Creek Culvert

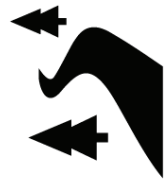
Opportunities

- Develop appropriate rehabilitation or replacement strategies to maintain the safe operation of the highway corridor for the current and future planning horizons
- Identify the ultimate footprint for the Highway 401 corridor to address current and future transportation needs



Investigations

In accordance with the MTO class environmental assessment process, this study includes engineering and environmental specialists who are carrying out background studies and site-specific investigations to support the development and the evaluation of alternatives and identify potential impacts and mitigation measures. The investigations for this study include, but are not limited to, the following:



Engineering Investigations

- Highway
- Traffic and Safety
- Bridge
- Drainage and Hydrology
- Geotechnical
- Foundations
- Electrical



Natural Environment Investigations

- Terrestrial
- Aquatic
- Species at Risk



Socio-Economic Investigations

- Heritage
- Noise and Air Quality
- Groundwater
- Archaeology & Heritage
- Contamination

Evaluation of Alternatives

Evaluation Process



Identify Criteria

Evaluation Criteria are established through:

- Public input
- Similar projects
- Provincial guidelines
- Existing conditions



Evaluate Alternatives

The evaluation criteria are utilized to develop advantages and disadvantages for each alternative and to undertake a qualitative evaluation of alternatives to identify the recommended plan.



The Highest Scoring Alternative

Evaluation Criteria

The following criteria were used to evaluate the Structure Alternatives and Widening Alternatives. Criteria were refined based on the input received at and following PIC 1, and used to identify the Technically Preferred Plan:

Engineering

- Consider total cost including utility relocations and property acquisition
- **Geometrics & Safety**
 - Design standards for provincial highways
 - Potential for collisions
 - Crossing road alignment
 - Pedestrian and cyclist accommodations
- **Constructability**
 - Construction techniques
 - Complexity of staging and detours
 - Traffic flow operations, including local access and out of way travel
- **Utilities**
 - Length of impacts to utilities

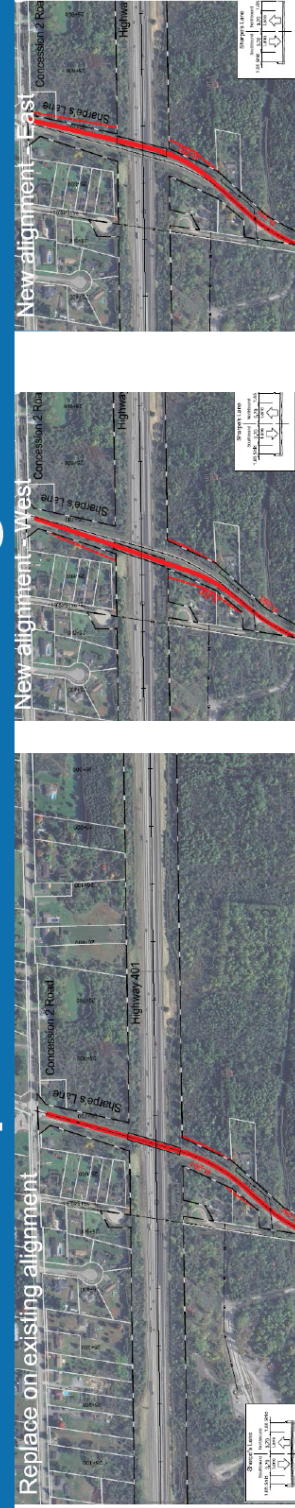
Social and Cultural Environment

- **Property**
 - Approximate area of impact to designated land uses
 - Approximate number of private properties potentially impacted
- **Groundwater**
 - Potential to impact groundwater recharge areas and highly vulnerable aquifers
- **Noise & Air Quality**
 - Potential change in traffic noise levels on surrounding residential dwellings
- **Archaeology & Built Heritage**
 - Possible impacts to areas having archaeological potential or built and cultural heritage potential
- **Contamination**
 - Potential to encounter contaminated soils/groundwater

Natural Environment

- **Terrestrial Ecosystem**
 - Area of impact to wildlife habitat
 - Area of impact to vegetated areas due to construction
- **Designated Areas**
 - Area of impact to significant ecological areas
- **Fish & Fish Habitat**
 - Number of water crossing impacted
 - Potential to impact fish/fish habitat
- **Species of Conservation Concern**
 - Potential to impact Species at Risk habitat

Evaluation of Structure Alternatives Sharpe's Lane Bridge



Factor/Criteria	1a Bridge Closed with Detour	1b Bridge Open with Single Lane	2 New Alignment - West	3 New Alignment - East
Highway Engineering <ul style="list-style-type: none"> • Geometrics & Safety • Constructability • Utilities • Cost • Traffic Operations 	<ul style="list-style-type: none"> • Retains existing alignment of Sharpe's Lane • Faster method of construction compared to staged construction with single lane • Lower construction staging cost compared to keeping bridge open during construction 	<ul style="list-style-type: none"> • Retains existing alignment of Sharpe's Lane • Maintain access across Highway 401 during construction 	<ul style="list-style-type: none"> • Maintains access across Highway 401 during construction with minimal impacts to traffic • Similar construction duration as closing the bridge • Similar construction staging cost as closing the bridge • No hydro pole impacts 	<ul style="list-style-type: none"> • Maintains access across Highway 401 during construction with minimal impacts to traffic • Similar construction duration as closing the bridge • Similar construction staging cost as closing the bridge
Social & Cultural Environment <ul style="list-style-type: none"> • Property • Noise & Air Quality • Archaeology & Built Heritage • Contamination • Groundwater 	<ul style="list-style-type: none"> • Less property required compared to Alternative 2 and 3 • Greater potential temporary impacts to local businesses compared to Alternative 1b due to detour 	<ul style="list-style-type: none"> • Less property required compared to Alternative 2 and 3 • Fewer potential temporary impacts to local businesses compared to Alternative 1a (does not require detour) 	<ul style="list-style-type: none"> • Requires more property compared to Alternatives 1 and 3 • Greater potential for noise impacts to residential properties compared to Alternative 1 • Greater potential impacts to areas of archaeological potential and potential contamination compared to Alternative 1 	<ul style="list-style-type: none"> • Requires more property compared to Alternative 1 • Greater potential for noise impacts to residential properties compared to Alternative 1 • Greater potential impacts to areas of archaeological potential and potential contamination compared to Alternative 1
Natural Environment <ul style="list-style-type: none"> • Terrestrial Ecosystem • Species of Conservation Concern • Designated Areas • Fish & Fish Habitat 	<ul style="list-style-type: none"> • Fewer potential impacts to forested areas compared to Alternatives 2 and 3 	<ul style="list-style-type: none"> • Fewer potential impacts to forested areas compared to Alternatives 2 and 3 	<ul style="list-style-type: none"> • Greater potential impacts to forested areas 	<ul style="list-style-type: none"> • Greater potential impacts to forested areas

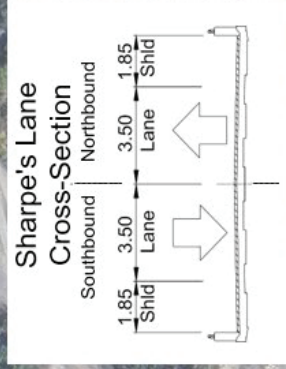
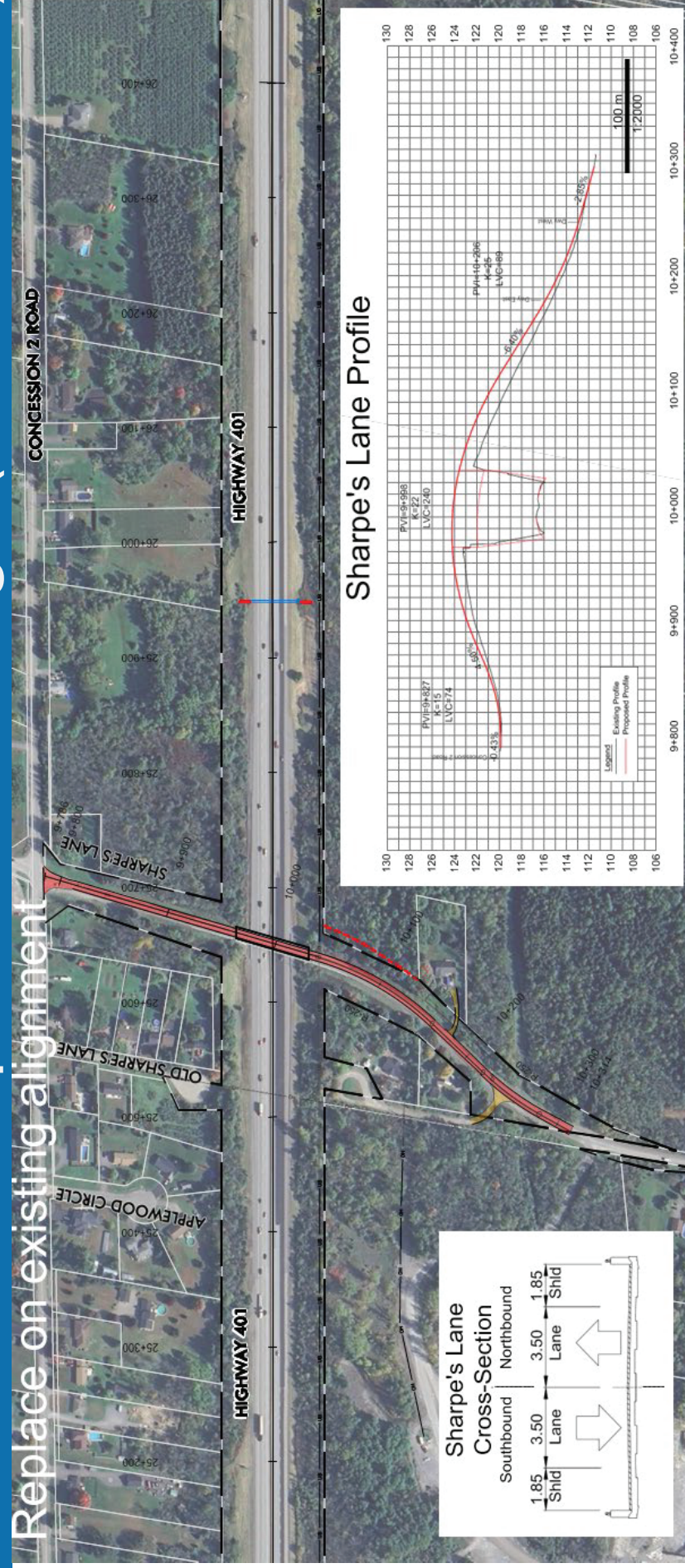


Least preferred

Most preferred

Preferred Sharpe's Lane Bridge (Alternative 1b)

Replace on existing alignment



This alternative is preferred because:

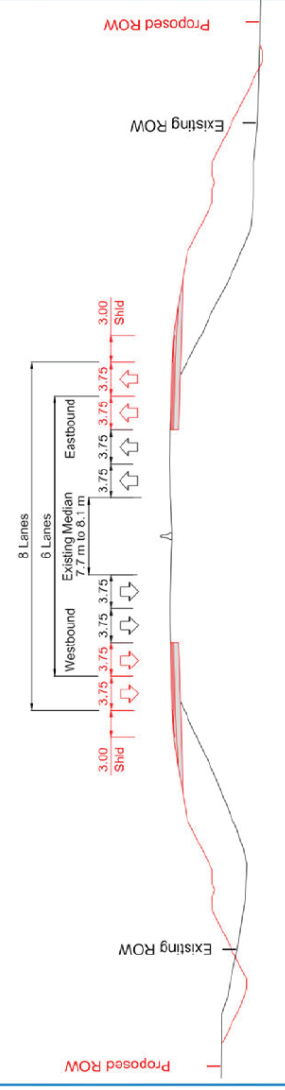
- Retains existing alignment of Sharpe's Lane, which minimizes environmental, property and utility impacts
- Maintains access across Highway 401 during construction
- Staged construction has fewer potential temporary impacts to local businesses compared to a full closure with detour

[Please click here to review a high-resolution version of Sharpe's Lane Alternatives](#)

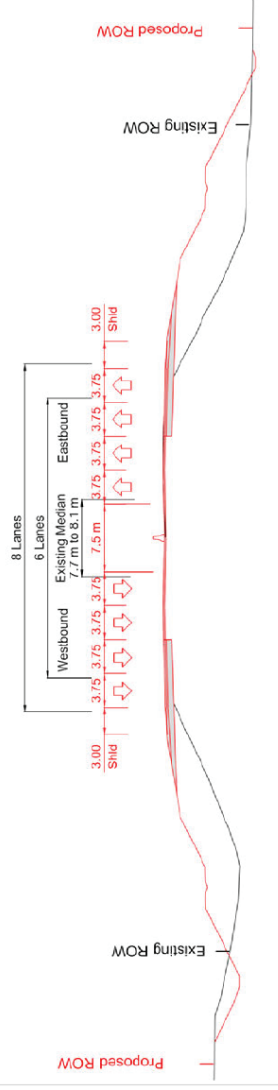


Preferred Highway 401 Cross-Section

1 Maintain Existing Median
Widen to the Outside



2 Standard Median (7.5 m)
Widen to the Inside and the Outside



Alternative 1 (Widen to Outside), is preferred because:

- Lower cost compared to Alternative 2
- Minimizes impacts to traffic during construction
- Retains the existing median with concrete barrier

The proposed cross-section for this study will match the cross-sections of the previously completed Highway 401 studies adjacent to this study.



Drainage Improvements Strategy



B Butler Creek Culvert

Culvert Improvement Alternatives

Replace with new culvert:

- + Long-term strategy
- + Accommodates interim (6-lane) Highway 401
- + Accommodates ultimate (8-lane) Highway 401
- Requires significant excavation



Culvert rehabilitation and retaining walls:

- Not a long-term strategy
- + Potential short-term strategy
- + Accommodates interim (6-lane) Highway 401
- Does not accommodate ultimate (8-lane) Highway 401
- Requires retaining walls

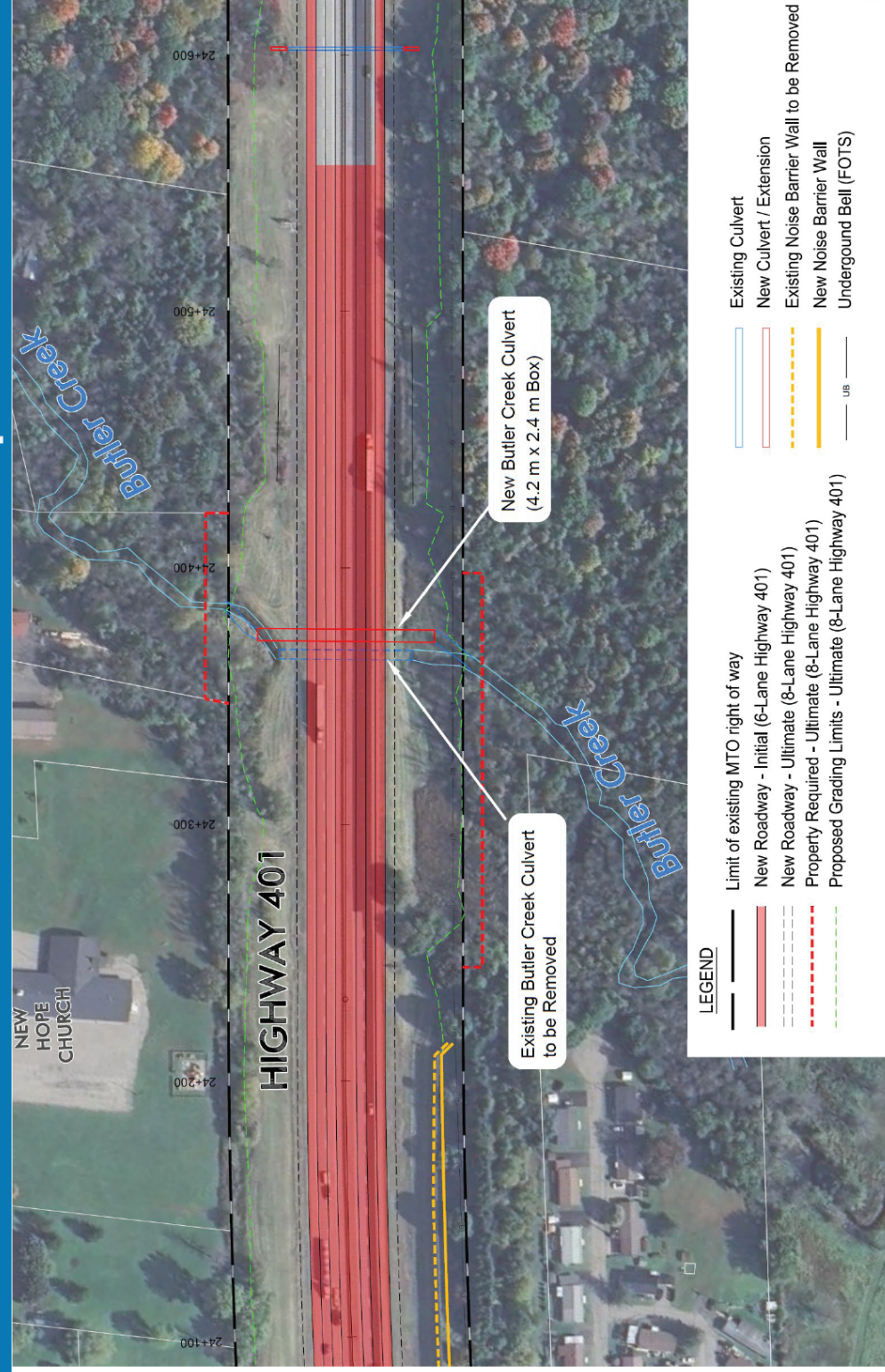
Culvert rehabilitation with extension:

- Not a long-term strategy
- + Potential short-term strategy
- + Accommodates interim (6-lane) Highway 401
- Does not accommodate ultimate (8-lane) Highway 401
- Future replacement of original culvert can be challenging



Butler Creek Culvert, Site 16X-0238/CO

Butler Creek Culvert Replacement



The Butler Creek Preferred Plan includes the existing culvert to be replaced with a new culvert at a new location east of the existing culvert. The Preferred Plan requires a minor realignment of Butler Creek, minor property impacts and significant excavation.

This alternative is preferred because:

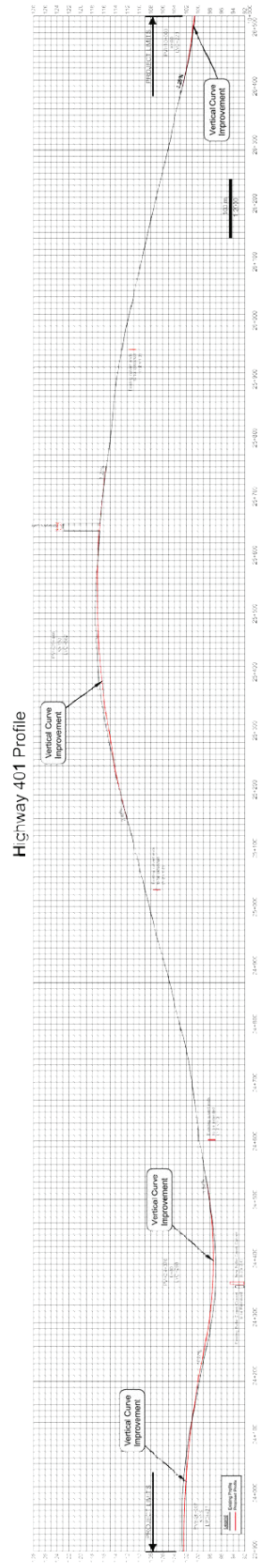
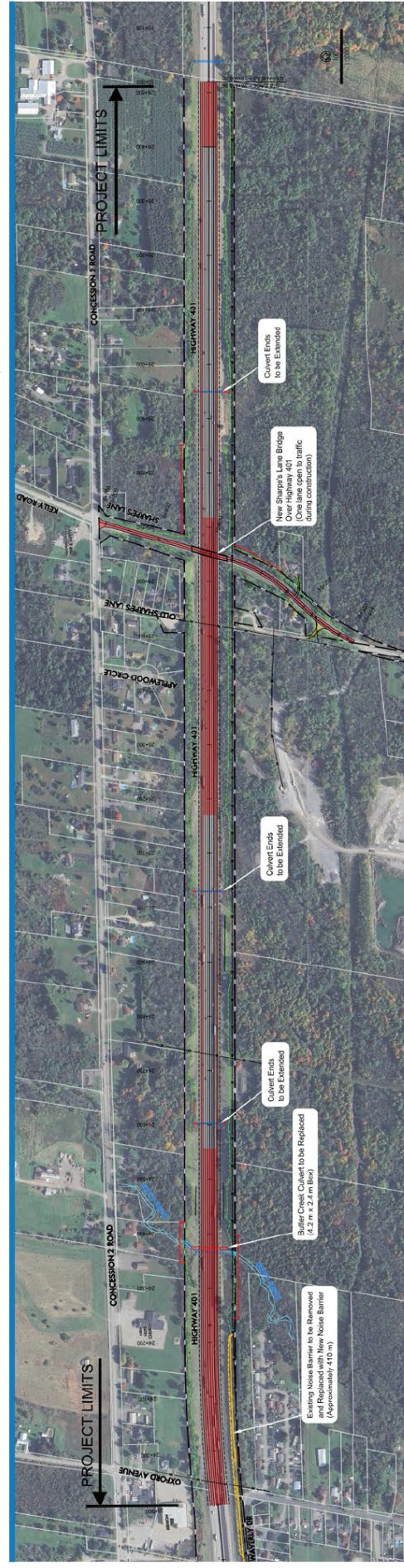
- The new culvert accommodates an interim 6-lane and ultimate 8-lane Highway 401 configuration
- Is a long-term strategy

Technically Preferred Plan

Highway 401 Technically Preferred Plan

Study Area Improvement Plan

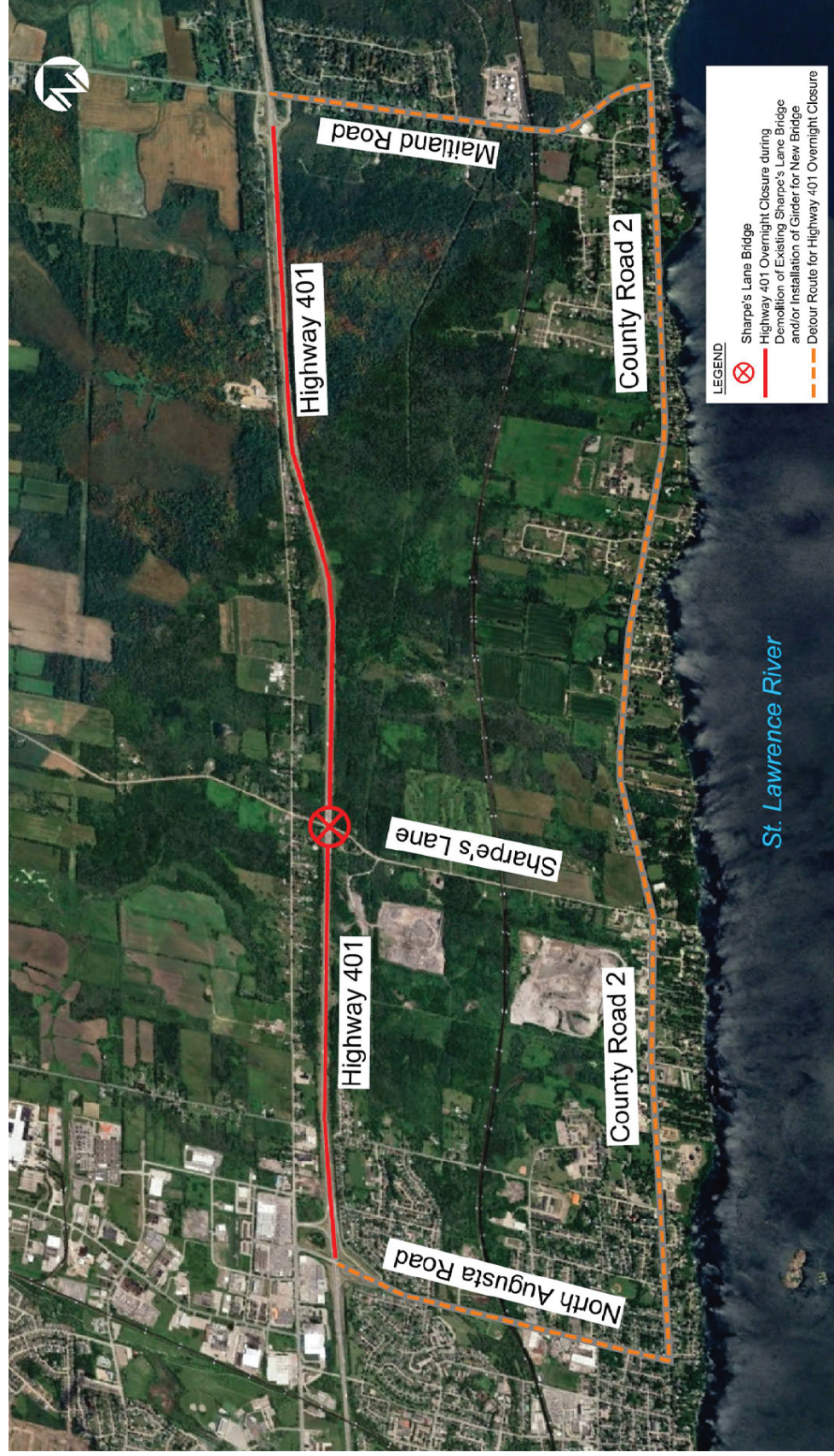
- LEGEND**
- Existing Lane
 - New Lane (1 Extension)
 - Existing Noise Barrier Wall to be Retained
 - New Noise Barrier Wall
 - New Noise Barrier Wall
 - Compacted Bit and Gravel
 - Concrete (for Lane Highway 401)
 - Underground Sewer (if 275)
 - Existing Lane
 - New Lane (1 Extension)
 - Existing Noise Barrier Wall to be Retained
 - New Noise Barrier Wall
 - New Noise Barrier Wall
 - Compacted Bit and Gravel
 - Concrete (for Lane Highway 401)
 - Underground Sewer (if 275)



Highway 401 Planning Study, East of Brockville GWP 4111-22-00

Please [click here](#) to review a high-resolution version of this drawing

Construction Staging and Traffic Management



Highway 401 Full Closures / Potential Detour Route

- A detour is required for the removal of the existing Sharpe's Lane bridge over the highway and potential girder installation for the new bridge
- Traffic on Highway 401 to use adjacent interchanges at Maitland Road, North Augusta Road, and County Road 2
- Duration and number of closures will be confirmed during Detail Design
- The final detour routes will be confirmed in consultation with the affected municipalities

Designation and MTO Permit Control Areas

Once the Technically Preferred Alternative has been confirmed, the Ministry of Transportation may designate lands as highway and acquire property prior to EA process completion. A designation enables the Ministry to provide route/corridor protection through the application of development control measures along the corridor and in the vicinity of the designated lands. For more information about highway corridor management, please visit: <https://www.ontario.ca/page/highway-corridor-management>.

Example of MTO Permit Control Areas: Controlled-Access Highways (CAH)



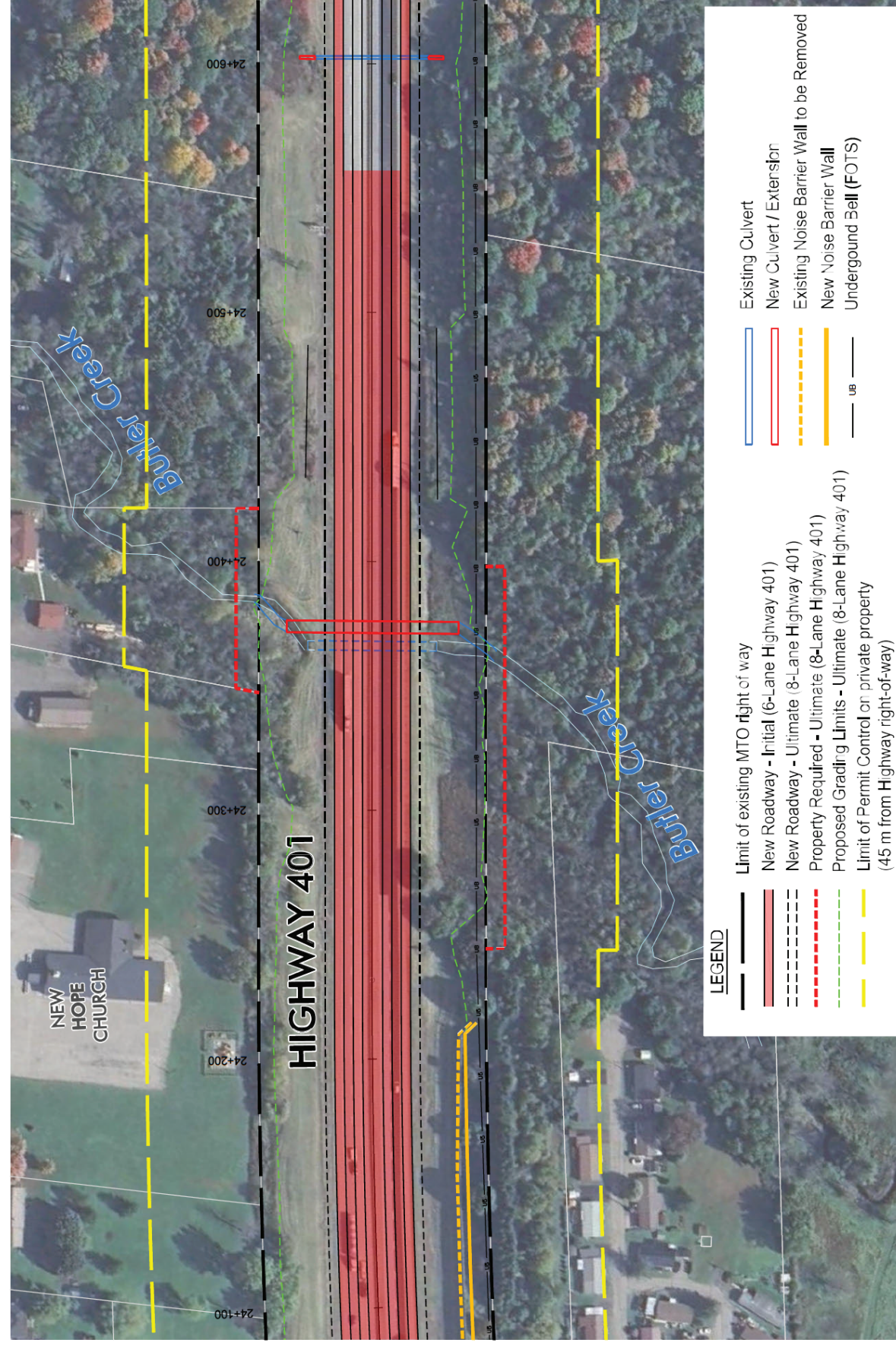
Any work on private property that is within 45 metres of the highway property or 395 metres from the centrepoint of an intersection or interchange requires approval from the MTO.

Any work on private property that is within 800 metres of the highway property and that may have a large impact on traffic, is also subject to approval from MTO.

For more details on requirements for permits, please visit: <https://www.ontario.ca/page/highway-corridor-management> or make an inquiry regarding a specific property: <https://www.hcms.mto.gov.on.ca/>

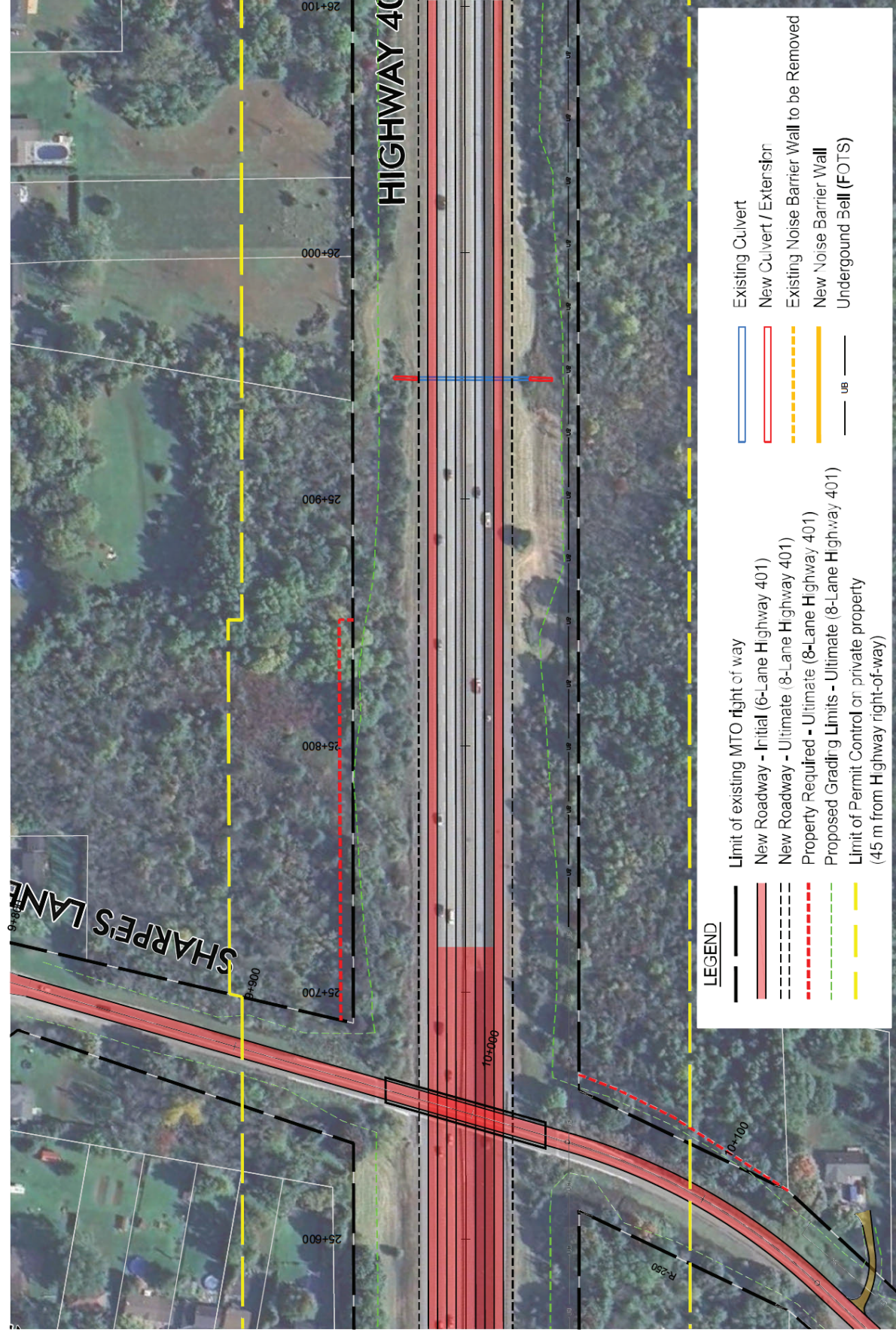
Note: Limit of the MTO right-of-way shown on the figure is for illustrative purposes only.

New MTO Permit Control Areas



- LEGEND**
- Limit of existing MTO right of way
 - New Roadway - Initial (6-Lane Highway 401)
 - New Roadway - Ultimate (8-Lane Highway 401)
 - - - Property Required - Ultimate (8-Lane Highway 401)
 - - - Proposed Grading Limits - Ultimate (8-Lane Highway 401)
 - Limit of Permit Control on private property (45 m from Highway right-of-way)
 - Existing Culvert
 - New Culvert / Extension
 - - - Existing Noise Barrier Wall to be Removed
 - New Noise Barrier Wall
 - Underground Bell (FOTS)

New MTO Permit Control Areas (continued)



Potential Impacts and Proposed Mitigation Measures

Impacts resulting from this project will be minimized to the extent possible. Investigations are ongoing and will continue during Detail Design to help confirm environmental impacts, refine mitigation measures, and support obtaining required permits and approvals.



Archaeological & Built Heritage Resources

A Stage 1 Archaeological Assessment was completed and it identified areas with potential for archaeological resources. A Stage 2 Archaeological Assessment will be undertaken for areas identified as retaining archaeological potential during Detail Design.



Property

Potential impacts to private property are an important factor and component of the evaluation of alternatives. The project team will work to minimize impacts to private properties to the extent possible. Potentially affected property owners have been contacted directly by the project team.



Noise Impacts

The potential changes in traffic noise associated with the Preferred Plan is currently being reviewed. A Noise Assessment is being completed in accordance with Provincial guidelines to determine if measures are required to mitigate potential increases in traffic noise (i.e., noise wall, landscaping, etc.). The results of the assessment will be documented in the TESR and made available for a comment period.



Traffic Operations

Construction activities are expected to impact traffic operations. Advanced notification will be provided to affected residents in advance of construction activities. Staging Plans will be finalized during detail design. Staging Plans will be finalized in consultation with municipalities and emergency service providers.



Natural Environment

Trees and vegetation will need to be removed and impacts will be minimized to the extent possible. A landscape design plan is being developed to identify opportunities for new plantings and landscape enhancements. Targeted species surveys will be completed during future studies for species that have the potential to be present. Mitigation measures to protect wildlife will be identified.

Next Steps

Following this Online Public Information Centre, the next steps in the Class EA process include:

- ✓ Review, consider and respond to comments received
- ✓ Confirm the Recommended Plan
- ✓ Confirm the potential impacts and proposed mitigation measures
- ✓ Prepare the Transportation Environmental Study Report (TESR)
- ✓ Issue Notice of Study Completion and file TESR for 30-day public comment period (Fall 2025)

[hwy401eastofbrockville.ca](http://www.hwy401eastofbrockville.ca)

Thank you for participating in this online PIC

Ways to provide your comments:

-  Complete the Online Comment Form
-  Email comments@hwy401eastofbrockville.ca
-  Contact by telephone:

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Visit the Study website:
<http://www.hwy401eastofbrockville.ca>

We would appreciate receiving your comments by August 30, 2025



Freedom of Information and Protection of Privacy Act
Comments and information regarding this study are being collected to satisfy the requirements of the Ontario Environmental Assessment Act, and in accordance with the Freedom of Information and Privacy Act. With the exception of personal information, all comments will become part of the public record.

Highway 401 East of Brockville, Planning, Preliminary Design, and Class EA – Online Public Information Centre 2 Presentation Script

Slide 1 (Introduction)

Hello, and thank you for joining us for the second online Public Information Centre for the Planning, Preliminary Design, and Class Environmental Assessment of Highway 401 East of Brockville.

This recorded presentation will provide you with an overview of this important Class Environmental Assessment (or Class E.A.) study.

Slide 2 (Purpose)

This is the Second Public Information Centre (P.I.C.) for this study. A copy of the information presented will be available for your review on the project website, www.hwy401eastofbrockville.ca.

The purpose of this online P.I.C. is to present and gather your feedback on:

- Evaluation of Alternatives
- Technically Preferred Plan and associated rationale
- Preliminary design for the Technically Preferred Plan
- Potential environmental impacts and proposed mitigation measures

This online P.I.C. also aims to answer questions you may have about this study.

Additional resources are available for you to download on the project website so that you can take the time to review the information in more detail. Comments can also be submitted on the project website.

Slide 3 (About the Project)

The Ontario Ministry of Transportation has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of Sharpe's Lane Bridge and Butler Creek Culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 750 m east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 kilometers), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

Highway 401 is a controlled-access, 400-series provincial highway that connects southwestern Ontario (Windsor) to Quebec, a total of 830 km. The highway, and the structures along it, were built in the 1950s and 1960s.

As part of this study, the project team will be reviewing existing conditions, developing and evaluating alternatives, identifying appropriate improvements, and developing environmental protection and mitigation measures. At the completion of the study, a recommended plan for the Highway 401 study area will be confirmed and designated (in other words, protected for future improvements).

This study builds off of previously completed studies on Highway 401 adjacent to this study area and aligns with the future highway footprint identified as part of those adjacent studies. Additional information for those studies is available at: www.highway401brockville.ca and www.highway401prescottmaitland.ca

Slide 4 (Class Environmental Assessment Process)

This study is being carried out under the requirements of the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024), which is approved under the Ontario Environmental Assessment Act for provincial transportation projects of a defined scope and magnitude.

The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process, which includes major improvements to existing provincial transportation facilities.

In 2019, modernization of the MTO Class EA (2000) was initiated to optimize the Class EA process by removing redundancies and expediting planning to facilitate a more streamlined implementation process. Amendments to the Class EA were approved in December 2023 and February 2024, and Class EA was renamed the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024). As part of the amendments, the EA process is now complete at the end of Preliminary Design, following the 30-day comment period for the Notice of Completion.

As part of the new MTO Class EA (2024), Preliminary Design is the primary opportunity to provide feedback on the project. Opportunities for meaningful consultation in future stages of the project will be minimal.

At the end of the study, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day comment period.

The goal of projects or activities covered under this Class E.A. is to provide a safe and effective transportation system while avoiding or minimizing negative environmental effects. The Class EA process consists of two main stages: Planning and Preliminary Design. The Planning stage focuses on developing a plan for how the project should be done, and the Preliminary Design stage focuses on 'roughing out' a design.

There are a number of steps to be completed as part of the Planning and Preliminary Design phases of the project, which allow for structured decision making to occur as the study progresses. These steps include:

- Data Collection – where available background information is reviewed, and field investigations are conducted as required to identify existing conditions in the study area
- Generating and evaluating alternatives – at this stage, Preliminary Design alternatives are developed to address structural needs, improve the highway safety and operations, and consider potential impacts to the existing natural, social, and cultural environment to identify a preferred plan
- Selection of a preferred plan – this step includes the identification of a preferred plan and proposes mitigation measures to address potential impacts to the environment
- Refine the preferred plan – as part of this step, the Preliminary Design of the preferred plan, including an implementation strategy, is proposed

- Reporting – once the information from the steps completed above have been gathered, considered, and finalized, the entire process is documented within a Transportation Environmental Study Report, or TESR, which will be posted on the project website for a 30-day public comment period. Notification of TESR Completion will be posted on the project website, within the *Brockville Recorder and Times* and *Brockville This Week* newspapers, and delivered via mail/email to those who have expressed an interest in this project
- Process Completion – this is an internal process that MTO uses to help to ensure that the Class EA requirements have been met before subsequent stages including detail design and construction begin

Consultation with the public, agencies, municipalities, and Indigenous communities, as well as identifying environmental protection measures are elements of the Planning and Preliminary Design phase of the project. Opportunities for consultation during this study are shown on this slide and include Public Information Centres, Municipal Technical Advisory Committee Meetings, responses to public notices, comments via the project website, and comments on the Transportation Environmental Study Report during the 30-day comment period.

The Implementation stage of the project, which includes Detail Design and construction of the proposed improvements, is not part of the Class E.A. process and will be undertaken in the future. As noted previously, opportunities for meaningful consultation during this stage of the project will be minimal as the Implementation stage is not part of the Class E.A. process. The timing of the Implementation stage of the project is not yet known.

Slide 5 (Online Public Information Centre 1 Summary)

The first online public information center was held between October 2, 2024 – November 8, 2024, to present and receive public input on the preliminary design alternatives and to address structural needs, improvements on highway safety and operations and consideration of potential impacts. Specifically, the PIC concentrated on the project background and the Class Environmental Assessment process, Transportation needs and opportunities in the study, preliminary structure alternatives for Sharpe's Lane Bridge, Highway 401 widening alternatives and preliminary existing study area conditions.

At and following the first P.I.C, we received comments on the following key themes:

- Potential impacts to property
- Potential impacts to safety and traffic
- Potential increase in noise
- Concerns with potential delays as a result of temporary closure of Sharpe's Lane Bridge during construction

Slide 6 (Project Overview)

As part of this study, bridge, drainage, and Highway 401 improvements are being considered. Sharpe's Lane Bridge is approaching the end of its service life and will need to be replaced. The bridge will need to accommodate the future Highway 401 footprints for the interim six and ultimate eight lanes.

In addition, the Butler Creek Culvert beneath Highway 401 is approaching the end of its service life and will need to be rehabilitated or replaced. The culvert will also need to accommodate the Highway 401 footprint for interim six and ultimate eight lanes.

Lastly, the study includes establishing the future Highway 401 footprints for the interim six lane and ultimate eight lane configurations.

Slide 7 (Problems and Opportunities)

The purpose of the study is to identify a recommended plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network. This study will include reviewing existing conditions, developing and evaluating alternatives, identifying a preferred plan, and developing environmental protection/mitigation measures.

At the completion of the study, a Recommended Plan for Highway 401 will be confirmed and designated (i.e., protected). The Ministry will be registering a designation of a proposed highway plan based on the future needs determined by this Planning and Preliminary Design study. A designation is a mechanism for the Ministry to provide route/corridor protection as well as impose development control measures along the corridor and on adjoining owners. This type of designation is most often used where lands are still under private ownership, sometimes years in advance of intended property acquisition and construction. The study has been initiated to address the following problems and opportunities:

Problems

- The bridge and structural culvert in the Study Area are nearing the end of their service life and will require rehabilitation and/or replacement in the near future.
- The existing Highway 401 platform cannot accommodate the traffic staging required to rehabilitate or replace the bridge and structural culvert

Opportunities

- Develop appropriate rehabilitation or replacement strategies to maintain the safe operation of the highway corridor for the current and future planning horizons
- Identify the ultimate footprint for the Highway 401 corridor to address current and future transportation needs

Slide 8 (Investigations)

In accordance with the MTO class environmental assessment process, this study includes engineering and environmental specialists who have carried out/are carrying out background studies and site-specific investigations to support the development and the evaluation of alternatives and identify potential impacts and mitigation measures. The investigations for this study include, but are not limited to, the following:

- Engineering investigations include highway, traffic and safety, bridge, drainage and hydrology, geotechnical, foundations and electrical.
- Natural Environment investigations include terrestrial ecosystems, fish and fish habitat and species at risk.

- Socio-Economic investigations include cultural and built heritage, noise and air quality, groundwater, archaeology and contamination.

Slide 9 (Evaluation of Alternatives)

The evaluation process includes identifying evaluation criteria through the input received during this study, the project team's experience in projects of this nature, provincial guidelines, and existing study area conditions.

As presented at P.I.C. 1, the Alternatives were assessed using the factors and criteria presented in this slide, including potential impacts and criteria pertaining to transportation and engineering, and socio-economic, natural, and cultural environments.

Comments received from agencies, stakeholders, Indigenous communities, and members of the public were incorporated into the evaluation.

The Technically Preferred Plan is selected as the aggregate of Preliminary Design alternatives that achieve the best overall balance of transportation engineering, individual environmental factor impacts, and overall environmental impact, taking into consideration the net environmental effects by applying conceptual mitigation measures.

Slide 10 (Evaluation of Structure Alternatives Sharpe's Lane)

As noted, this study includes the evaluation of the design modifications to Sharpe's Lane Bridge. A range of alternatives for replacing this bridge have been developed, including replacement of the bridge on its existing alignment with either the bridge closed with a detour or the bridge open with a single lane of traffic, on a new alignment to the west, and on a new alignment to the east.

Based on the Highway Engineering Evaluation Criteria, Alternative 1B was found to be moderately preferred because it:

- Retains the existing alignment of Sharpe's Lane and
- Maintains access across Highway 401 during construction

Based on the Social and Cultural Environment Evaluation Criteria, Alternative 1B was found to be the most preferred because it:

- Requires less property compared to Alternative 2 and 3
- Results in fewer potential temporary impacts to local businesses compared to Alternative 1A (does not require detour)
- Results in fewer impacts to areas of archaeological, built heritage and contamination potential compared to Alternatives 2 and 3

Based on the Natural Environment Evaluation Criteria, Alternative 1B was most preferred because it has:

- Fewer potential impacts to forested areas compared to Alternatives 2 and 3

Based on the evaluation, replacement of the bridge on its existing alignment with the bridge open with a single lane (Alternative 1B) was selected as most preferred.

Slide 11 (Preferred Sharpe's Lane Bridge)

The preferred alternative for Sharpe's Lane Bridge is Alternative 1B because it:

- Retains existing alignment of Sharpe's Lane, which minimizes environmental, property and utility impacts
- Maintains access across Highway 401 during construction
- Has fewer potential temporary impacts to local businesses compared to a full closure with detour

Slide 12 (Preferred Highway 401 Cross-Section)

Two alternatives for improvements to Highway 401 were developed. Alternative 1 includes maintaining the existing median and widening the highway to the outside, and Alternative 2 includes a standard median and widening the highway to both the inside and outside. The preliminary screening determined that only Alternative 1 should be carried forward as it retains the existing median and concrete barrier, minimizes impacts to traffic during construction, and has a lower cost compared to Alternative 2. Alternative 2 was screened out as it requires more complicated traffic staging compared to Alternative 1, requires reconstruction of the median including concrete barrier, and has a higher cost compared to Alternative 1.

Alternative 1 has been selected as the preferred option because it:

- Has a lower cost compared to Alternative 2
- Minimizes impacts to traffic during construction
- Retains the existing median with concrete barrier
- Is consistent with the proposed cross section of the previously completed Highway 401 studies on either side of the study limits.

Slide 13 (Drainage Improvements Strategy)

Butler Creek Culvert is the only structural culvert in the Study Area. Three improvement strategies were considered for the existing culvert, including replacement with a new culvert, culvert rehabilitation and construction of retaining walls, and culvert rehabilitation with a culvert extension.

Replacement of the existing culvert with a new culvert is the preferred alternative as it will accommodate both the interim 6-lane and ultimate 8-lane Highway 401 configuration but requires significant excavation.

Slide 14 (Butler Creek Culvert Replacement)

The Butler Creek Replacement of the existing culvert with a new culvert alternative is preferred because it:

- Retains the existing alignment of Sharpe's Lane, which minimizes environmental, property and utility impacts
- Maintains access across Highway 401 during construction
- Staged construction has fewer potential temporary impacts to local businesses compared to a full closure with detour

Slide 15 (Technically Preferred Plan)

The Technically Preferred Plan includes:

- Replacement of the existing Sharpe's Lane bridge over Highway 401
- Replacement of the existing Butler Creek culvert
- Widening of Highway 401 to an interim 6-lane and ultimate 8-lane cross-section
- Drainage improvements
- Replacement of existing noise barrier wall along Highway 401

Slide 16 Construction Staging and Traffic Management

Highway 401 Full Closures / Potential Detour Route:

A detour is required for the removal of the existing Sharpe's Lane bridge over the highway and potential girder installation for the new bridge. Traffic on Highway 401 will use adjacent interchanges at Maitland Road, North Augusta Road, and County Road 2 during the closures. The duration and number of closures will be confirmed during Detail Design. The final detour routes will be confirmed in consultation with the affected municipalities

Slide 17 Designation and MTO Permit Control Areas

Once the Technically Preferred Alternative has been confirmed, the Ministry of Transportation may designate lands as highway and acquire property prior to EA process completion. A designation enables the Ministry to provide route/corridor protection through the application of development control measures along the corridor and in the vicinity of the designated lands.

For more information about highway corridor management, please visit: <https://www.ontario.ca/page/highway-corridor-management>.

Any work on private property that is within 45 metres of the highway property or 395 meters from the centrepoint of an intersection or interchange requires approval from the MTO.

Any work on private property that is within 800 metres of the highway property and that may have a large impact on traffic, is also subject to approval from MTO.

For more details on requirements for permits, please visit: <https://www.ontario.ca/page/highway-corridor-management> or make an inquiry regarding a specific property: <https://www.hcms.mto.gov.on.ca/>

Note: Limit of the MTO right-of-way shown on the figure is for illustrative purposes only.

Slide 18 New MTO Permit Control Area 1:

Changes to the existing MTO Permit Control Area along Highway 401 will be required to accommodate the Preferred Plan at the Butler Creek Culvert, which include minor impacts to private properties, as shown on the figure presented.

Slide 19 New MTO Permit Control Area 2:

Changes to the existing MTO Permit Control Area along Highway 401 will be required to accommodate the Preferred Plan at Sharpe's Lane, which include minor impacts to private properties, as shown on the figure presented.

Slide 20 (Potential Impacts and Proposed Mitigation Measures)

Impacts resulting from this project will be minimized to the extent possible. Preliminary mitigation measures have been identified in this slide and will be further refined during Detail Design. Investigations are ongoing and will continue to help confirm environmental impacts, refine mitigation measures, and support obtaining required permits and approvals.

With regard to the Archeological Resources a Stage 1 Archeological Assessment was completed and it identified areas with potential for archaeological resources. A Stage 2 Archeological Assessment will be undertaken during Detail Design for areas identified as retaining archaeological potential.

Potential impacts to private property are an important factor and component of the evaluation of alternatives. The project team will work to minimize impacts to private properties to the extent possible.

Potentially affected property owners have been contacted directly by the project team. With regard to noise impacts, the potential changes in traffic noise associated with the Preferred Plan is currently being reviewed. A Noise Assessment is being completed in accordance with Provincial guidelines to determine if measures are required to mitigate potential increases in traffic noise (i.e., noise wall, landscaping, etc.) and any required mitigation will be included in the final Recommended Plan.

Construction activities are expected to impact traffic operations. Advanced notification will be provided to affected residents in advance of construction activities. Staging Plans will be finalized during detail design. Staging Plans will be finalized in consultation with municipalities and emergency service providers.

No significant fish habitat was identified in the study area as part of the Fish and Fish Habitat Existing Conditions Report. Potential impacts to fish and fish habitat may occur as a result of the Preferred Plan (including the replacement of the Butler Creek Culvert). Appropriate protection measures including in-water timing windows (i.e., no in-water work from March 15 to June 30) will be proposed. Detailed protection measures will be documented in the Transportation Environmental Study Report. Lastly, vegetation removal will be minimized to the extent possible. A landscape design plan is being developed to identify opportunities for new plantings and landscape enhancements. Targeted species surveys will be completed during future studies for sensitive species that have the potential to be present. Protection and mitigation measures to protect wildlife will be identified and documented in the Transportation Environmental Study Report.

Slide 21 (Next Steps)

The Project Team will complete the following after this Public Information Centre:

- Review, consider, and respond to the comments received
- Confirm the Recommended Plan
- Confirm the potential impacts and proposed mitigation measures
- Prepare the Transportation Environmental Study Report for the project
- Issue a Notice of Study Completion and file the Transportation Environmental Study Report for a 30-day comment period, which is tentatively scheduled for Fall 2025

Slide 22 (Thank you)

On behalf of the Ontario Ministry of Transportation, we would like to thank you for your interest in this study, and for taking the time to participate in this online Public Information Centre. Your input is very important to us, and there are several ways to provide your comments.

We encourage you to review all PIC materials available on the project website, as well as to submit comments or questions to the project team.

If you have any comments or questions, please do not hesitate to contact the project team via email at: comments@hwy401eastofbrockville.ca, or by contacting one of the project team members listed on this slide by telephone.

We would appreciate receiving any comments or questions you may have by August 30, 2025. Thank you again for taking the time to participate in this online PIC.

Highway 401 Planning Study East of Brockville ONLINE PUBLIC INFORMATION CENTRE 2

SUMMARY REPORT

GWP 4111-22-00



Prepared for:
Ministry of Transportation
Eastern Region
1355 John Counter Boulevard
Kingston ON K7L 5A3

Prepared by:
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek, ON L8J 0B4

September 15, 2025

**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 2**

Table of Contents

1.0	Introduction.....	3
2.0	Purpose	3
3.0	Location, Date, and Time	3
4.0	Notification.....	3
5.0	Reference Materials and Handouts.....	4
6.0	Format	5
7.0	Participation.....	5
8.0	Comments Received	6
9.0	Next Steps	8

List of Tables

Table 1: Statistics Gathered from Project Website	6
Table 2: Public Input Received and Responses Provided/Action Taken	6

List of Appendices

APPENDIX A Notification Materials	9.1
APPENDIX B PIC 1 Materials	1
APPENDIX C Online Participation Statistics.....	1
APPENDIX D Comments Received	1
APPENDIX E Indigenous Engagement.....	1
APPENDIX F Newspaper Articles.....	1

1.0 Introduction

The Preliminary Design and Class Environmental Assessment (Class EA) Study for Highway 401 East of Brockville has two planned Public Information Centres (PIC). The second of two PICs took place from July 17, 2025, to August 30, 2025, and was held in an online format on the project website. A recorded presentation and comment forms were provided on the website as part of the online PIC. Hard copies of the presentation were also available on request.

2.0 Purpose

PICs form part of the overall consultation plan for the project and are designed to involve stakeholders early and throughout the study to identify concerns and provide opportunities for input.

The purpose of PIC 2 was to provide the public and stakeholders with an opportunity to review the online public information 1 summary, evaluation of alternatives, technically preferred plan, and potential impacts and proposed protection and mitigation measures.

A Transportation Environmental Study will be prepared and issued for a 30-day comment period in early 2026.

3.0 Location, Date, and Time

PIC 2 consisted of a recorded presentation that was hosted on the study website (<http://www.hwy401eastofbrockville.ca>) beginning on July 17, 2025. Comments related to the online PIC were requested by August 30, 2025. Hardcopies of the presentation and transcript were available for mailout for those unable to access the PIC 2 materials provided on the study website.

4.0 Notification

The Notice of PIC 2 provided information about the PIC, including the purpose, and date and time that the presentation would become available on the study website.

The notice and letter were sent via email to the MPP for Leeds-Grenville-Thousand Islands and Rideau Lakes on July 2, 2025, and to external agencies, businesses, and members of the public on July 8, 2025. A hard copy of the notice was also sent to 2,126



HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE ONLINE PUBLIC INFORMATION CENTRE 2

residents and business owners during the week of July 8, 2025, via Canada Post Admail.

Notices to advise the public of the second PIC were published in the following newspapers:

- *Brockville Recorder & Times* (in English, on July 17, 2025)
- *Brockville This Week* (in English, on July 17, 2025)

Letters were sent to the following Indigenous communities/organizations via email on July 2, 2025, to notify them of the second PIC:

- Métis Nation of Ontario
- Mohawks of Akwesasne

The Notice of PIC 2 and associated letters provided information about the PIC, including its purpose and the date that the presentation would become available on the study website.

Comments regarding the Online PIC 2 presentation were requested to be submitted by August 30, 2025.

Copies of the newspaper notices and the notification materials are provided in **Appendix A**.

5.0 Reference Materials and Handouts

The following displays were presented as part of the online PIC 2 presentation:

- Welcome
- Online Public Information Centre 2
- About the Project
- Class Environmental Assessment Process
- Online Public Information Centre 1 Summary
- Project Overview
- Problems and Opportunities
- Investigations
- Evaluation of Alternatives
- Evaluation of Structure Alternatives Sharpe's Lane Bridge
- Preferred Sharpe's Lane Bridge (Alternative 1B)
- Preferred Highway 401 Cross Section
- Drainage Improvements Strategy
- Butler Creek Culvert Replacement

HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE ONLINE PUBLIC INFORMATION CENTRE 2

- Technically Preferred Plan
- Construction Staging and Traffic Management
- Designation and MTO Permit Control Areas
- New MTO Permit Control Areas
- New MTO Permit Control Areas (continued)
- Potential Impacts and Proposed Mitigation Measures
- Next Steps
- Thank you for Participating

A copy of the PIC displays are included in **Appendix B**.

6.0 Format

As noted, the PIC was hosted online via the study website <http://www.hwy401eastofbrockville.ca>. A recorded presentation, including the transcript and displays, were available for review, and members of the public were encouraged to submit comments and questions or to contact the project team to provide feedback on the study. Hardcopies of the transcript and displays were available via courier to those unable to access the online format, where requested.

The information available as part of the online PIC, including a copy of the presentation, transcript, and comment forms is provided in **Appendix B**.

7.0 Participation

Statistics were gathered during the online PIC period from July 17, 2025 to August 30, 2025, to determine the number of viewers of the online presentation. The website statistics were broken down into the following two categories:

- 1) **Views** – The number of pageviews on a website and screenviews on an app. For this category, the same person visiting the website multiple times during the PIC time period can be counted multiple times.
- 2) **Active users** – The total number of users that accessed the website during the PIC period. For this category, the same person visiting the website multiple times during the PIC is counted once.

The data gathered for each category is illustrated in Table 1.



Table 1: Statistics Gathered from Project Website

Views	Active Users
45	23

A total of 23 users to the website were recorded during the PIC time period.

A copy of the raw statistics of the participants is provided in **Appendix C**.

8.0 Comments Received

In total, five (5) comments were received through email. The online version of the comment sheet was provided as a link within the PIC 2 presentation. Copies of the comments are presented in **Appendix D**. All personal information has been removed, consistent with the *Freedom of Information and Protection of Privacy Act*.

An overview of the comments received during the PIC comment period is provided in Table 2.

Table 2: Public Input Received and Responses Provided/Action Taken

Comment	Response Provided and/or Action Taken
PIC 2 Notice	
<ul style="list-style-type: none"> Asked why they received a notice of the Public Information Centre 2 and whether their property is impacted 	<p>Thank you for your email,</p> <p>Please note that the Highway 401 Planning Study East of Brockville (GWP 4111-22-00) as well as the previously completed Highway 401 Brockville Planning Study (GWP 4003-19-00) do not impact your property, and you would have received the notice for general information purposes only.</p> <p>Please don't hesitate to contact us should you have any further questions.</p>
Impacted Properties	

Comment	Response Provided and/or Action Taken
<ul style="list-style-type: none"> Comment thanking the project team for the impacted property owner letter and asked whether the team needs to access or expand into their land 	<p>A call was held with the property owner and the MTO and was recorded as follows:</p> <p>The property owner is open to the property acquisition process.</p> <p>MTO let them know that the 401 is a controlled access highway and access is limited to interchanges, so access to their property would not be approved.</p> <p>The property owner noted they are happy to help where and want to meet with MTO Corridor representatives to discuss what options they have for access. MTO will send them a separate e-mail in this regard.</p> <p>MTO let them know that this is a planning study and that they would not be looking to acquire the property for at least five years.</p>
Noise Barriers	
<ul style="list-style-type: none"> Comment asking if a noise barrier is planned from Sharpe's Lane to Applewood Circle 	<p>Thank you for your comment regarding the implementation of a noise wall.</p> <p>The project team is completing a Noise Impact Assessment as part of this study. This noise analysis will follow the Ministry of Transportation's Environmental Guide for Noise (the MTO Guide) published in 2022 which complies with the requirements of the Ontario Environmental Assessment Act. This assessment includes a review of previous studies relevant to the area, along with an analysis of Noise Sensitive Areas (NSAs), proposed developments, traffic data, construction equipment and schedules, and any applicable local or municipal noise bylaws.</p> <p>The team is evaluating road traffic noise impacts carefully, and any proposed noise mitigation measures will be assessed for both technical and economic feasibility. Recommendations from the noise assessment will be included in the final plan.</p>



9.0 Next Steps

Responses to all formal questions and comments received as a result of the second PIC were prepared and forwarded to the commenting party. Consultation is ongoing, and all relevant agencies will be notified at future project milestones.

Following the second PIC, a Transportation Environmental Study Report (TESR) will be prepared to document the study and the Recommended Plan, and it will be made available for a 30-day comment period. Any concerns brought to the attention of the Project Team will be responded to and an attempt to reach a resolution will be made.

APPENDIX A Notification Materials





NOTICE OF PUBLIC INFORMATION CENTRE 2
PRELIMINARY DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT
Highway 401 Planning Study East of Brockville, GWP 4111-22-00

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville. The purpose of the study is to identify a Recommended Plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network.

THE PROCESS

This study is following the approved planning process for a Group 'B' project under the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways, 2024 (Class EA). The project team, which includes representatives from Stantec and MTO, have completed environmental and engineering field investigations and are seeking input from individuals, organizations, and Indigenous communities. Upon completion of Preliminary Design, the project team will prepare a Transportation Environmental Study Report (TESR) and issue a Notice of Completion. The project team will include comments received, responses to the comments and any supporting materials in a record of consultation.

Once a Technically Preferred Alternative has been identified, the Ministry of Transportation may designate lands as highway and acquire property prior to EA process completion. A designation enables the Ministry to provide route/corridor protection through the application of development control measures along the corridor and in the vicinity of the designated lands. For more information about highway corridor management, please visit: <https://www.ontario.ca/page/highway-corridor-management>.

ONLINE PUBLIC INFORMATION CENTRE 2

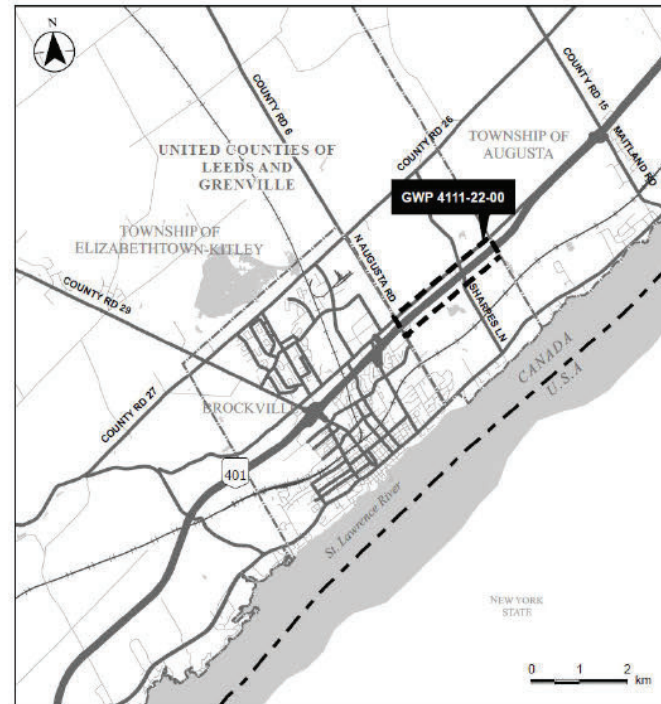
Public Information Centre (PIC) 1 was offered on the study website between October 2, 2024, and November 8, 2024, to present and solicit feedback on the study background, existing study area conditions, preliminary improvement alternatives, the evaluation process, and next steps in the Class EA process.

PIC 2 is currently being planned to present and gather feedback on the evaluation of alternatives, the technically preferred plan, and next steps in the Class EA process.

The second online PIC will be available for your review on the study website at www.hwy401eastofbrockville.ca starting July 17, 2025. A recorded presentation will be available as part of the online PIC until August 30, 2025.

COMMENTS

Additional details will be available on the project website, www.hwy401eastofbrockville.ca. We encourage you to participate in the study and to provide comments through the website or by contacting the project team members listed below. If you have questions about the study or would like to be added to the mailing list, please contact:



Gregg Cooke, P.Eng.
Consultant Project Manager
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek ON L8J 0B4
Tel: (905) 381-3227
Call Collect: (905) 385-3234
comments@hwy401eastofbrockville.ca

David Brake
Senior Project Manager
Ministry of Transportation Eastern Region
1355 John Counter Boulevard, Postal Bag 4000
Kingston ON K7L 5A3
Tel: (613) 893-3031
Fax: (613) 540-5106
comments@hwy401eastofbrockville.ca

If you have any accessibility requirements in order to participate in this project, please contact one of the Project Team members listed above. Comments and information are being collected to assist the MTO in meeting the requirements of the Ontario Environmental Assessment Act. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record. Pour obtenir des renseignements en français, composer le 1-(905)-381-3224 (Tim Belliveau), Courriel: tim.belliveau@stantec.com.

Highway 401 Planning Study in the Township of Elizabethtown-Kitley (GWP 4111-22-00)
External Agencies - Project Mailing List

Title	First Name	Last Name	Position	Organization	Address	Address 2	City	Province	Postal Code	Telephone	Fax	Email	
External Agencies and Stakeholders													
Federal Agencies													
	Umar	Naveed		CN Rail								Umar.Naveed@cn.ca	
Provincial Agencies													
	Dan	Minkin	Heritage Planner	Ministry of Citizenship and Multiculturalism	401 Bay Street		Toronto	ON	M7A 0A7	416-769-7533		Dan.Minkin@ontario.ca	
	Chris	Luchini	Heritage Planner	MCIM						416-305-9171		chris.luchini@ontario.ca	
	Chris	Parsons	Open Land Heritage Planning Unit	MCIM						416-690-1027		chris.parsons@ontario.ca	
	Liam	Smythe	Heritage Planner	MCIM						416-301-4797		liam.smythe@ontario.ca	
	Tracy L	Adam	District Manager	Ministry of Natural Resources and Forestry, Peterborough District	300 Water Street	1st Floor B	Peterborough	ON	K9J 3C7	513-847-5116		tracy.adam2@ontario.ca	
	Ms. Catherine	Warren	Conservation Program Advisor	Ministry of Natural Resources and Forestry, Peterborough District	300 Water Street	2nd Floor S	Peterborough	ON	K9J 3C7	705-772-9012		catherine.warren@ontario.ca	
	Jan	Cropano	Environmental Resource Planner & EA Coordinator	Ministry of the Environment, Conservation and Parks	125 St. Clair Ave W	7th Fl	Toronto	ON	M4V 1P4	913-911-6930		jan.cropano@ontario.ca	
	Brandon	Norman	Management Biologist - Species at Risk Branch	MECP Biologi						705-914-6950		brandon.norman@ontario.ca	
	Charmaine	McGinnis		Ministry of Indigenous Affairs	160 Bloor Street East	9th Floor	Toronto	ON	M7A 2E6			charmaine.mcginnis@ontario.ca	
Municipal													
	Rob	Nolan	Chief Administrator	Township of Elizabethtown-Kitley	8544 New Dublin Rd		Address	ON	R9E 1A0	613-345-7480 x 237		rob.nolan@ekwp.ca	
	Laura	Shergal	Deputy Chief	Township of Elizabethtown-Kitley	8544 New Dublin Rd		Address	ON	R9E 1A0	613-345-7480 x 218		laura.shergal@ekwp.ca	
	Barah	Flaeger	Planning Administrative Assistant	Township of Elizabethtown-Kitley	8544 New Dublin Rd		Address	ON	R9E 1A0	613-345-7480 x 217		barah@ekwp.ca	
	Michael	Cranny	Planner	Township of Elizabethtown-Kitley	8544 New Dublin Rd		Address	ON	R9E 1A0	613-345-7480 x 216		micranny@ekwp.ca	
	Cassidy	Public Works Manager	Township of Elizabethtown-Kitley	8544 New Dublin Rd		Address	ON	R9E 1A0	613-345-7480 x 216		ccassidy@ekwp.ca		
	Raymond	Callery	CMO	United Counties of Leeds and Grenville	25 Central Ave. W.	Suite 100	Brockville	ON	K9V 4N6	613-342-3840 x 2301		ray.callery@leeds.on.ca	
	Rob	Keene	Director of Public Works	United Counties of Leeds and Grenville	25 Central Ave. W.	Suite 100	Brockville	ON	K9V 4N6	613-342-3840 x 2412		rob.keene@leeds.on.ca	
	Shirley	Wardell-Lean	Public Works - Admin Assistant	United Counties of Leeds and Grenville	25 Central Ave. W.	Suite 100	Brockville	ON	K9V 4N6	613-342-3840 x 2413		shirley.wardell@leeds.on.ca	
	Cherie	Mills	Manager of Planning Services	United Counties of Leeds and Grenville	25 Central Ave. W.	Suite 100	Brockville	ON	K9V 4N6	613-342-3840 x 2413		cherie.mills@leeds.on.ca	
	Bill	Gay	Asst. Director of Public Works	United Counties of Leeds and Grenville	25 Central Ave. W.	Suite 100	Brockville	ON	K9V 4N6	613-342-3840 x 2426		bill.gay@leeds.on.ca	
	Christa	Hollroy	Planner	United Counties of Leeds and Grenville	25 Central Ave. W.	Suite 100	Brockville	ON	K9V 4N6	613-342-3840 x 2427		christa.hollroy@leeds.on.ca	
	Mr	Shawn	Allen	Supervisor of Engineering	City of Brockville	P.O. Box 5000	Brockville	ON	K9V 7A5	613-342-8772 x 3223		shawn@brockville.com	
	Matthew	Cook	Supervisor of Transportation Services	City of Brockville	1 King Street West	P.O. Box 5000	Brockville	ON	K9V 7A5	613-342-8772 x 3025		mcook@brockville.com	
	Mr	Phil	Wood	Director of Operations	City of Brockville	P.O. Box 5000	Brockville	ON	K9V 7A5			wood@brockville.com	
	Ms	Stephanie	Executive Assistant to the Mayor and City Manager	City of Brockville	1 King Street West	P.O. Box 5000	Brockville	ON	K9V 7A5	613-342-8772 x 4430		stephanie@brockville.com	
	Tracy	McGinnis	Chief Planning Officer	City of Brockville	1 King Street West	P.O. Box 5000	Brockville	ON	K9V 7A5	613-342-8772 x 4449		tracy@brockville.com	
Conservation Authorities													
	Mr	Tom	Buehler	Manager, Conservation Lands	Galopaki	Region Conservation Authority	1641 Perth Road	Cambridge	ON	R0H 1S0	613-546-4228 ext. 240	613-547-6474	tbuehler@orca.ca
					11740 Lawrence Park Commission	Mississauga	ON	R0C 1A0	1-800-431-2233			tom@lakeshore.ca	
					Ontario Parks	Ontario Parks	115 Brockville Rd	Brockville	ON	R9B 1L0	613-342-9977		tom@ontarioparks.org
Local Elected Representatives													
	Mr	Frank	Mayor	MPO - Minister of Municipal Affairs and Housing	11 Broad Street		Unit 205	Brockville	ON	K9V 8C4	613-342-9902	613-342-9901	frank@leeds.on.ca
	Mr	Frank	Mayor	Township of Elizabethtown-Kitley	8544 New Dublin Rd		Address	ON	R9E 1A0	613-342-9906		frank@ekwp.ca	
Emergency Services													
	Chief	Jeff	Sergeant	Fire Chief	Township of Elizabethtown-Kitley Fire Department	44 Main St E	Lin	ON	R9E 1M0	613-498-2680		jeff@ekwp.ca	
	Deputy Fire Chief	Mike	Deputy Fire Chief	Deputy Fire Chief	Township of Elizabethtown-Kitley Fire Department	44 Main St E	Lin	ON	R9E 1M0	613-498-2680		mikew@ekwp.ca	
	Chief	Rob	Boorman	Fire Chief	Augusta Fire Department	1622 County Road 15	Lin	ON	R9E 1P0	613-498-2680		rob@ekwp.ca	
	Ma	Ashleigh	Trickey	Fire Department Coordinator	Augusta Fire Department	1622 County Road 15	Maitland	ON	R9E 1P0	613-925-4231 ext 202		ashley@augusta.ca	
	Simon	Hardy	Department Commander	Department Commander	Township of Elizabethtown-Kitley Fire Department	44 Main St E	Lin	ON	R9E 1M0	613-498-2680		simon@ekwp.ca	
	Imp	Simon	Hardy	Ontario Provincial Police - Leeds County (Brockville)	1102 County Road 15	P.O. Box 636	Brockville	ON	R9V 5V6	613-345-1750	613-345-2202	simon.hardy@oppc.ca	
	Imp	Simon	Hardy	Ontario Provincial Police - Rideau Lakes	2781 Highway 15	Unit 101	Parliamnt	ON	R0C 1V0	613-272-8602	613-283-3451	simon.hardy@oppc.ca	
	Chief	Mike	Prior	Fire Chief	Department Commander	814 County Road 3	Box 255	Lansdowne	ON	R9E 1L6	613-898-6000	613-899-5178	simon.hardy@oppc.ca
	Mr	Jeff	Cass	Leeds and 1000 Islands Fire Department	1433 Prince Street		Lansdowne	ON	K9E 1L6	613-328-8335		jeff@leedsand1000islands.on.ca	
	School Boards / School Transportation	Jeff	Cass	Leeds Grenville Paramedic Service	25 Central Ave W	Suite 100	Brockville	ON	K9V 4N6	613-342-3840 x 2404		jeff.cass@leeds.on.ca	
School Boards / School Transportation													
	Mr	Ron	Ferguson	Director of Education	Upper Canada District School Board	225 Central Ave W	Brockville	ON	R9V 8X1	855-568-1980		ron.ferguson@ucdsb.on.ca	
	Mr	Laurie	Compton	Director of Education	Catholic District School Board of Eastern Ontario	4155 Highway 43	Kemptville	ON	R0C 1A8	613-869-7757 x 3077	613-258-5967	lcompton@cdsbe.on.ca	
	Mr	Mark	McIntosh	Director of Education	Conestoga College of Applied Arts and Sciences	1000 Lakeshore Street	Orono	ON	R1L 1A1	613-735-2555		mark.mcintosh@conestoga.ca	
	Mr/Madam			Student Transportation Services of Eastern Ontario	104 Commerce Dr	P.O. Box 1179	Prescott	ON	R9E 1T0	613-925-0022	613-925-0024	transportation@stseo.ca	
Other Stakeholders													
	Ms	Dan	Robinson	Executive Director	Brockville & District Chamber of Commerce	3 Market Street W	Suite 1	Brockville	ON	R9V 7L2	613-342-8553 x 101		d.robinson@brockvillechamber.com
	Ms	Amy	Wyle	Economic Development Manager	Leeds and Grenville Economic Development	32 West Street	Suite 300	Brockville	ON	N9V 4P9	613-342-3840 x 5365		amy.wyle@leeds.on.ca
	Mr/Madam	Jonathan	Blackburn	Co-Owner/CEO	Ontario Trucking Association	255 Queen Road	Toronto	ON	M9V 1H9	416-529-7571 x 249		john.blackburn@ontariotruck.org	
	Mr/Madam	Carly	Carson	General Manager	Coach Canada	2615 Fabre Drive	Unit 101	Peterborough	ON	K9J 7B1	705-748-6411	705-748-2450	carly.carson@coachcanada.com
	Mr	Ylva	Brooks	Director of Communications and Stakeholder Relations	Ontario Federation of Agriculture	100 Stone Road West, Suite 206	Guelph	ON	N1G 5L3	519-821-6883	519-821-6819	ylva.brooks@ofa.on.ca	
	Mr/Madam	Corinna	Davis	President	Leeds County Federation of Agriculture	100 Stone Road West, Suite 206	Guelph	ON	N1G 5L3	519-821-6883		corinna.davis@ofa.on.ca	
	Mr	Bennie	Davis	President	Grenville Snowmobile Association	4901 Charlotteville Rd, RR#4	Prescott	ON	R9E 1T0	613-925-2281		bennie@gsa.on.ca	
	Mr/Madam	Terri	White	President	Grenville County Historical Society Inc	460 Railway Avenue	Box 992	Prescott	ON	R9E 1T0	8871-925-9489		terri@huntspears.ca
	Mr				College Canada Inc.	1884 Hwy 42	Brockville	ON	R9V 9C2	613-342-0292		terri.white@stantec.com	

From: Young, Rachel (Waterloo) <Rachel.Young2@stantec.com>

Sent: Tuesday, July 8, 2025 9:57 AM

To: comments@hwy401eastofbrockville.ca

Subject: Notice of Public Information Centre 2- Highway 401 Planning Study East of Brockville (GWP 4111-22-00)

Good morning,

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and to identify the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of this email is to advise you of the second online Public Information Centre (PIC) planned for this study. As indicated within the attached notice, the online PIC will be available for your review on the study website at www.hwy401eastofbrockville.ca, starting on July 17th, 2025. A recorded presentation will be available as part of the online PIC. The project team is requesting that comments be submitted by August 30th, 2025.

The information presented at this online PIC will provide you with an opportunity to review and comment on the evaluation of alternatives, the technically preferred plan, and next steps in the Class EA process.

You are encouraged to visit the project website, www.hwy401eastofbrockville.ca, to obtain current project information and to submit comments to the project team.

If you are unable to view the online PIC and would like further information regarding the study, please contact either the undersigned or one of the project team members named in the enclosed information.

Thank you,

Rachel Young (she/her)
Transportation/Environmental Planner

Direct: (519) 575-4228

Email: Rachel.young2@stantec.com

Stantec
100-300 Hagey Boulevard
Waterloo ON N2L 0A4



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Caution: This email originated from outside of Stantec. Please take extra precaution.

Attention: Ce courriel provient de l'extérieur de Stantec. Veuillez prendre des précautions supplémentaires.

Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.

Ministry of Transportation

Engineering Program
Delivery East

Project Delivery East

1355 John Counter
Boulevard
Postal Bag 4000
Kingston ON K7L 5A3

Tel: 613-893-3031

Ministère des Transports

Livraison du programme
d'ingénierie dans l'Est
Zone d'exécution du projet
de l'Est

1355, boulevard John
Counter CP/Service de
sacs 4000
Kingston ON K7L 5A3

Tél : 613-893-3031



July 2nd, 2025

Mr. Steve Clark, MPP
Leeds-Grenville-Thousand Islands and Rideau Lakes
100 Strowger Blvd.
Suite 101
Brockville ON K6V 5J9
steve.clark@pc.ola.org

Reference: Highway 401 Planning Study East of Brockville (GWP 4111-22-00) - Notice of Online Public Information Centre 2

Dear Steve Clark:

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design and Class Environmental Assessment (Class EA) study on Highway 401 for the replacement and/or rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of this letter is to advise you of the second online Public Information Centre (PIC) planned for this study. As indicated within the attached notice, the online PIC will be available for your review on the study website at www.hw401eastofbrockville.ca, starting on July 17, 2025. A recorded presentation will be available as part of the online PIC.

The information presented at this online PIC will provide you with an opportunity to review and comment on the evaluation of alternatives, the technically preferred plan, and next steps in the Class EA process.

A copy of the Notice of PIC is attached and will be placed in the Brockville Recorder and Times and Brockville This Week on July 17, 2025.

**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 2**

0 APPENDIX B
PIC 2 Materials
September 15, 2025

This project is being completed as a Group 'B' project under the MTO Class Environmental Assessment (Class EA) for Provincial Transportation Facilities and Municipal Expressways (2024). This study includes opportunities for public engagement, including two Public Information Centres.

Upon completion of Preliminary Design, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day public comment period. Notices will be published in local newspapers to advise the public of the TERS public comment period.

You are encouraged to visit the project website, www.hwy401eastofbrockville.ca to obtain additional study information and/or to submit comments to the study team. If you have any questions regarding the study, please contact the undersigned, or one of the individuals named in the attached notice.

Sincerely,



David Brake
Senior Project Manager
Ministry of Transportation | Ontario Public Service
1355 John Counter Boulevard, Postal Bag 4000
Kingston ON K7L 5A3
613-893-3031 | dave.brake@ontario.ca

Attachment: Notice of Public Information Centre 2

**APPENDIX B
PIC 2 Materials**





hwy401eastofbrockville.ca

Highway 401

East of Brockville

Planning, Preliminary Design, and
Class Environmental Assessment Study

GWP 4111-22-00



Welcome to

Online Public Information Centre 2

The purpose of this online PIC is to present and gather your feedback on the:

- Evaluation of Alternatives
- Technically Preferred Plan and associated rationale
- Preliminary design for the Technically Preferred Plan
- Potential environmental impacts and proposed mitigation measures

This online PIC also aims to answer questions you may have about the project.

Other information related to this PIC that is available on the project website includes:

- A PDF copy of this presentation
- A PDF copy of the technically preferred plan
- Online comment form

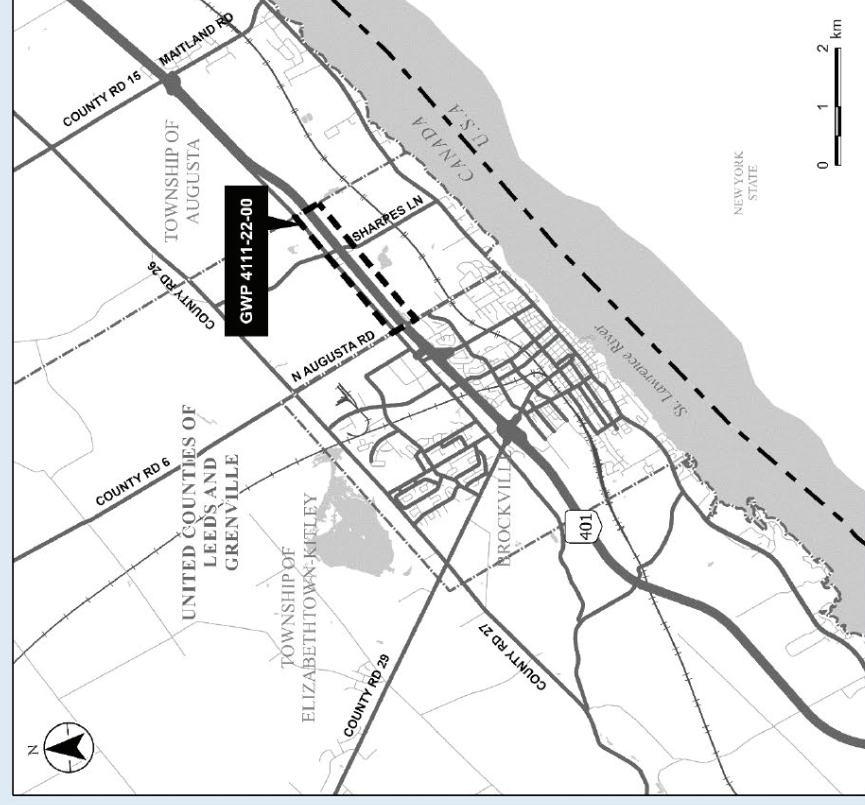


About The Project

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Planning, Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of Sharpe's Lane bridge and Butler Creek culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

Highway 401 is a controlled-access 400-series provincial highway that connects southwestern Ontario (Windsor) to Quebec, a total of 830 km. The highway, and the structures along it, were built in the 1950s and 1960s.

As part of this study, the project team reviewed existing conditions, developed and evaluated alternatives, identified appropriate improvements, and developed environmental protection and mitigation measures. At the completion of the study, a recommended plan for the Highway 401 study area will be confirmed and designated (i.e. protected).



Ontario  

Class Environmental Assessment Process Group 'B' Projects

Environmental Assessment Process

This study is being carried out under the requirements of the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024), which is approved under the Ontario Environmental Assessment Act for provincial transportation projects of a defined scope and magnitude.

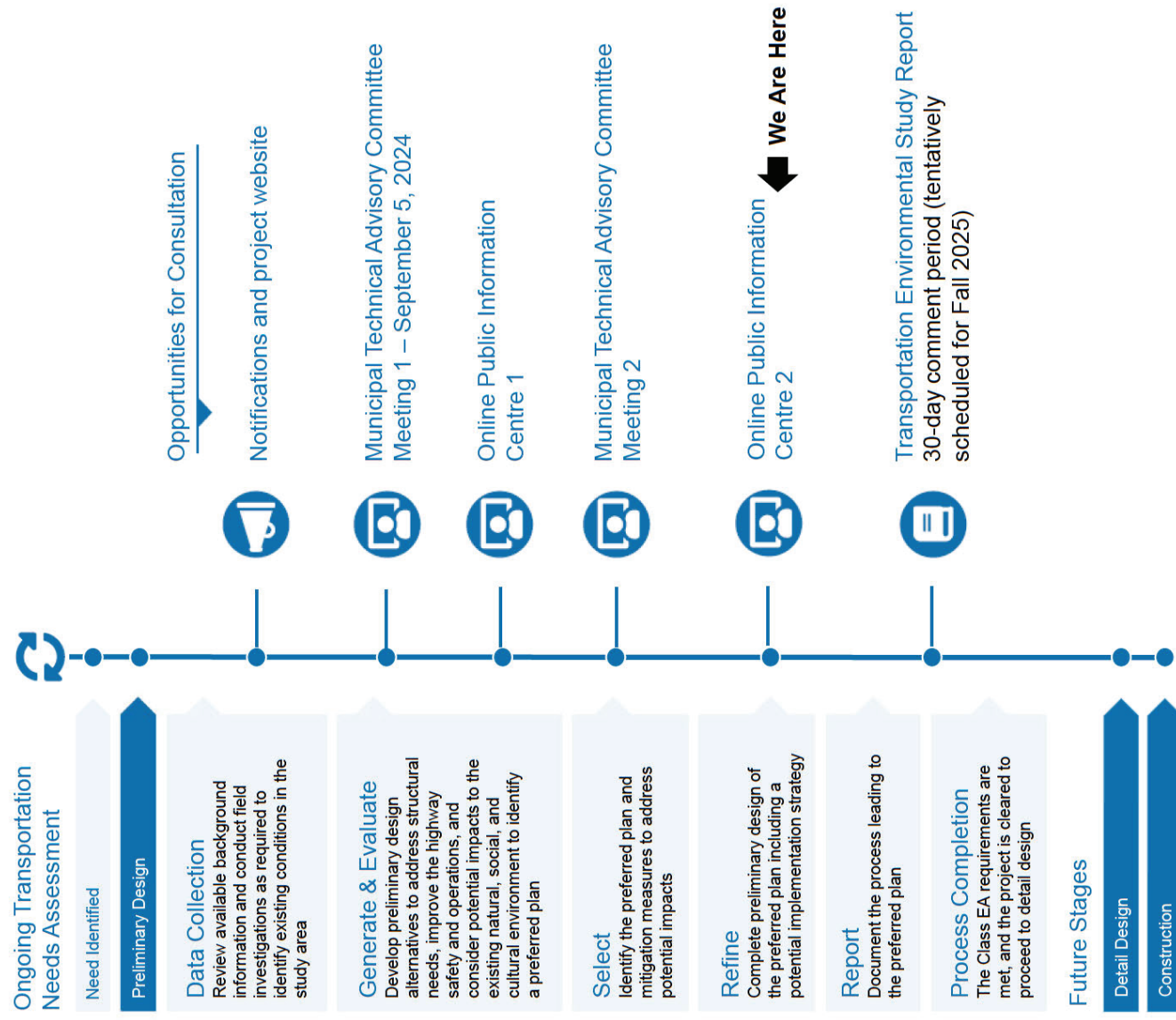
The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process, which includes major improvements to existing provincial transportation facilities.

In 2019, modernization of the MTO Class EA (2000) was initiated to optimize the Class EA process by removing redundancies and expediting planning to facilitate a more streamlined implementation process. Amendments to the Class EA were approved in December 2023 and February 2024, and Class EA was renamed the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024). As part of the amendments, the EA process is now complete at the end of Preliminary Design, following the 30-day comment period for the Notice of Completion.

As part of the new MTO Class EA (2024), Preliminary Design is the primary opportunity to provide feedback on the project. Opportunities for meaningful consultation in future stages of the project will be minimal.

At the end of the study, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day comment period.

Please [click here](#) to access a copy of the [MTO Class EA document](#)



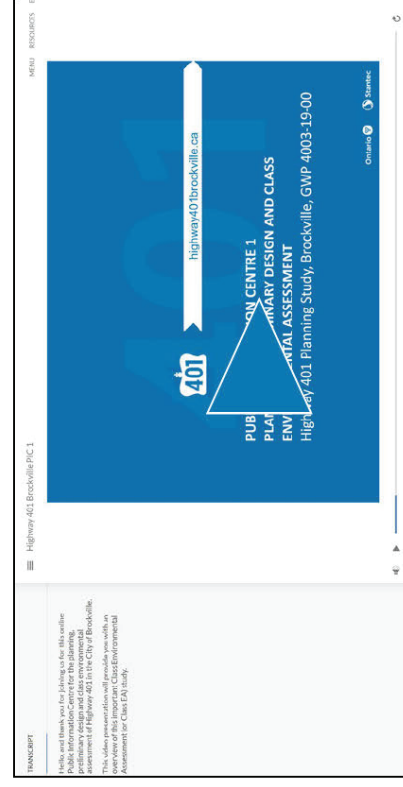
Online Public Information Centre 1 Summary

Online Public Information Centre 1 was held between October 2, 2024 - November 8, 2024, to present and gather feedback on:

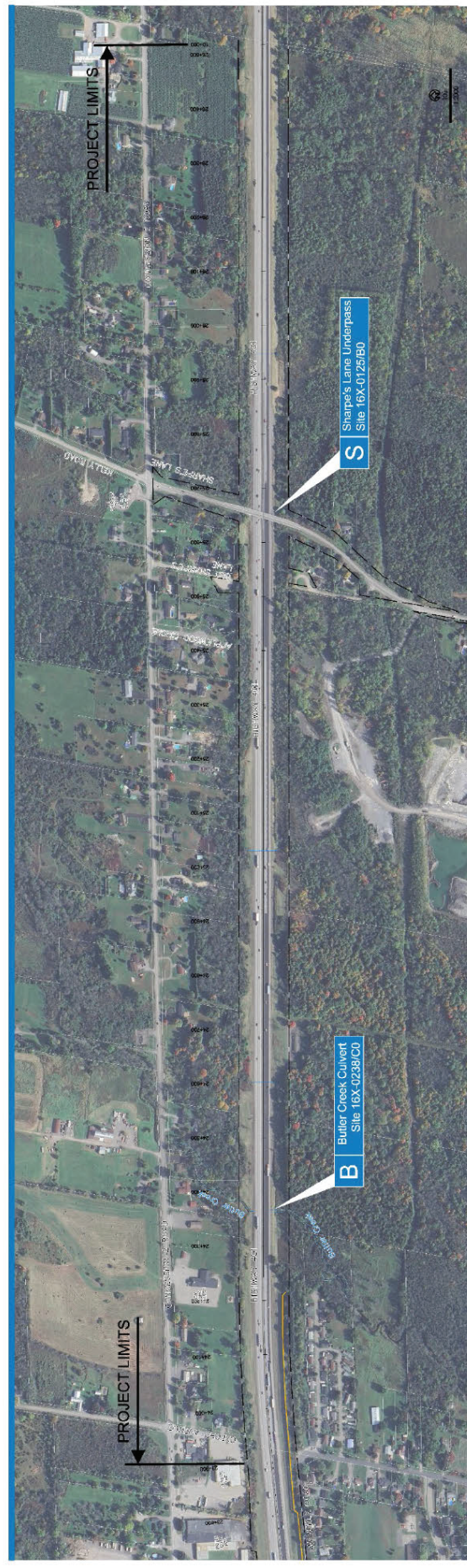
- Project background and the Class Environmental Assessment process
- Transportation needs and opportunities in the study area
- Preliminary structure alternatives for Sharpe's Lane Bridge
- Highway 401 widening alternatives
- Preliminary existing study area conditions

What did we hear?

- Potential impacts to property
- Potential impacts to safety and traffic
- Potential increases in noise
- Concerns with potential delays as a result of temporary closure of Sharpe's Lane Bridge during construction



Project Overview



Bridge Improvement

The Sharpe's Lane bridge is approaching the end of its service life and will need to be replaced. The bridge will need to accommodate the future Highway 401 footprint for interim six lanes and ultimate eight lanes.

> for more details, see:

Bridge Improvement Alternatives

S Sharpe's Lane bridge over Highway 401



Highway 401 Improvements

The study includes establishing the future Highway 401 footprint for the interim six lanes and ultimate eight lanes configurations.

> for more details, see:

Highway 401 Alternatives



Drainage Improvements

There is a structural culvert at Butler Creek which is approaching the end of its service life and will need to be rehabilitated or replaced. The culvert will need to accommodate the future Highway 401 footprint for interim six lanes and ultimate eight lanes. Existing structural and non-structural culverts are shown on the plan above.

> for more details, see:

Drainage Improvements

B Butler Creek Culvert

LEGEND

- Existing Highway 401 right-of-way
- Existing Structural Culvert
- Existing Non-Structural Culvert
- Existing Noise Barrier Wall

[Please click here to review a high-resolution version of the Project Overview plan](#)

Problems And Opportunities

The purpose of the study is to identify a recommended plan that addresses current and future transportation needs in the study areas as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network. This study will include reviewing existing conditions, developing and evaluating alternatives, identifying a preferred plan, and developing environmental protection/mitigation measures.

At the completion of the study, a Recommended Plan for Highway 401 will be confirmed and designated (i.e., protected). The Ministry will be registering a designation of a proposed highway plan based on the future needs determined by this Planning and Preliminary Design study. A designation is a mechanism for the Ministry to provide route/corridor protection as well as impose development control measures along the corridor and on adjoining owners. This type of designation is most often used where lands are still under private ownership, sometimes years in advance of intended property acquisition and construction.

This study has been initiated to address the following problems and opportunities:

Problems

- Sharpe's Lane Bridge and Butler Creek Culvert are nearing the end of their service life and will require rehabilitation and/or replacement in the near future
- The existing Highway 401 platform cannot accommodate the traffic staging required to rehabilitate or replace Sharpe's Lane Bridge and Butler Creek Culvert

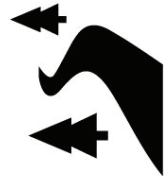
Opportunities

- Develop appropriate rehabilitation or replacement strategies to maintain the safe operation of the highway corridor for the current and future planning horizons
- Identify the ultimate footprint for the Highway 401 corridor to address current and future transportation needs



Investigations

In accordance with the MTO class environmental assessment process, this study includes engineering and environmental specialists who are carrying out background studies and site-specific investigations to support the development and the evaluation of alternatives and identify potential impacts and mitigation measures. The investigations for this study include, but are not limited to, the following:



Engineering Investigations

- Highway
- Traffic and Safety
- Bridge
- Drainage and Hydrology
- Geotechnical
- Foundations
- Electrical



Natural Environment Investigations

- Terrestrial
- Aquatic
- Species at Risk



Socio-Economic Investigations

- Heritage
- Noise and Air Quality
- Groundwater
- Archaeology & Heritage
- Contamination

Evaluation of Alternatives

Evaluation Process

Identify Criteria

Evaluation Criteria are established through:

- Public input
- Similar projects
- Provincial guidelines
- Existing conditions

Evaluate Alternatives

The evaluation criteria are utilized to develop advantages and disadvantages for each alternative and to undertake a qualitative evaluation of alternatives to identify the recommended plan.

The Highest Scoring Alternative

Evaluation Criteria

The following criteria were used to evaluate the Structure Alternatives and Widening Alternatives. Criteria were refined based on the input received at and following PIC 1, and used to identify the Technically Preferred Plan:

Engineering

- Consider total cost including utility relocations and property acquisition
- **Geometrics & Safety**
 - Design standards for provincial highways
 - Potential for collisions
 - Crossing road alignment
 - Pedestrian and cyclist accommodations
- **Constructability**
 - Construction techniques
 - Complexity of staging and detours
 - Traffic flow operations, including local access and out of way travel
- **Utilities**
 - Length of impacts to utilities

Social and Cultural

Environment

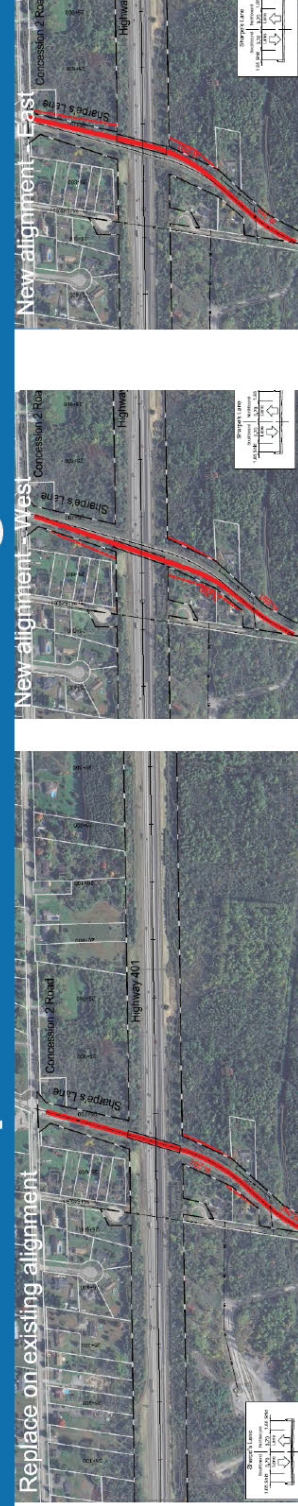
- **Property**
 - Approximate area of impact to designated land uses
 - Approximate number of private properties potentially impacted
- **Groundwater**
 - Potential to impact groundwater recharge areas and highly vulnerable aquifers
- **Noise & Air Quality**
 - Potential change in traffic noise levels on surrounding residential dwellings
- **Archaeology & Built Heritage**
 - Possible impacts to areas having archaeological potential or built and cultural heritage potential
- **Contamination**
 - Potential to encounter contaminated soils/groundwater

Natural

Environment

- **Terrestrial Ecosystem**
 - Area of impact to wildlife habitat
 - Area of impact to vegetated areas due to construction
- **Designated Areas**
 - Area of impact to significant ecological areas
- **Fish & Fish Habitat**
 - Number of water crossing impacted
 - Potential to impact fish/fish habitat
- **Species of Conservation Concern**
 - Potential to impact Species at Risk habitat

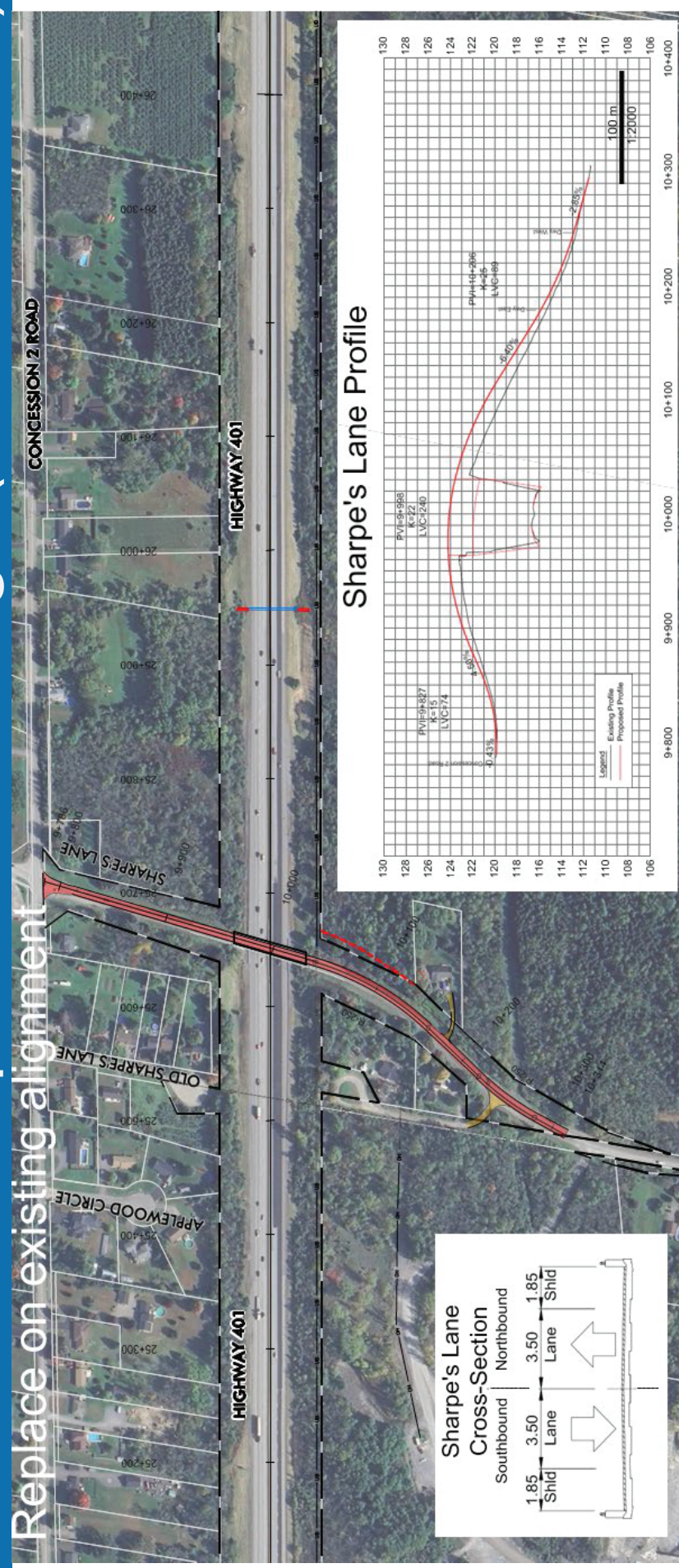
Evaluation of Structure Alternatives Sharpe's Lane Bridge



Factor/Criteria	1a Bridge Closed with Detour	1b Bridge Open with Single Lane	2 New Alignment - West	3 New Alignment - East
Highway Engineering <ul style="list-style-type: none"> • Geometrics & Safety • Constructability • Utilities • Cost • Traffic Operations 	<ul style="list-style-type: none"> • Retains existing alignment of Sharpe's Lane • Faster method of construction compared to staged construction with single lane • Lower construction staging cost compared to keeping bridge open during construction 	<ul style="list-style-type: none"> • Retains existing alignment of Sharpe's Lane • Maintain access across Highway 401 during construction 	<ul style="list-style-type: none"> • Maintains access across Highway 401 during construction with minimal impacts to traffic • Similar construction duration as closing the bridge • Similar construction staging cost as closing the bridge • No hydro pole impacts 	<ul style="list-style-type: none"> • Maintains access across Highway 401 during construction with minimal impacts to traffic • Similar construction duration as closing the bridge • Similar construction staging cost as closing the bridge
Social & Cultural Environment <ul style="list-style-type: none"> • Property • Noise & Air Quality • Archaeology & Built Heritage • Contamination • Groundwater 	<ul style="list-style-type: none"> • Less property required compared to Alternative 2 and 3 • Greater potential temporary impacts to local businesses compared to Alternative 1b due to detour 	<ul style="list-style-type: none"> • Less property required compared to Alternative 2 and 3 • Fewer potential temporary impacts to local businesses compared to Alternative 1a (does not require detour) 	<ul style="list-style-type: none"> • Requires more property compared to Alternatives 1 and 3 • Greater potential for noise impacts to residential properties compared to Alternative 1 • Greater potential impacts to areas of archaeological potential and potential contamination compared to Alternative 1 	<ul style="list-style-type: none"> • Requires more property compared to Alternative 1 • Greater potential for noise impacts to residential properties compared to Alternative 1 • Greater potential impacts to areas of archaeological potential and potential contamination compared to Alternative 1
Natural Environment <ul style="list-style-type: none"> • Terrestrial Ecosystem • Species of Conservation Concern • Designated Areas • Fish & Fish Habitat 	<ul style="list-style-type: none"> • Fewer potential impacts to forested areas compared to Alternatives 2 and 3 	<ul style="list-style-type: none"> • Fewer potential impacts to forested areas compared to Alternatives 2 and 3 	<ul style="list-style-type: none"> • Greater potential impacts to forested areas 	<ul style="list-style-type: none"> • Greater potential impacts to forested areas

Preferred Sharpe's Lane Bridge (Alternative 1b)

Replace on existing alignment



This alternative is preferred because:

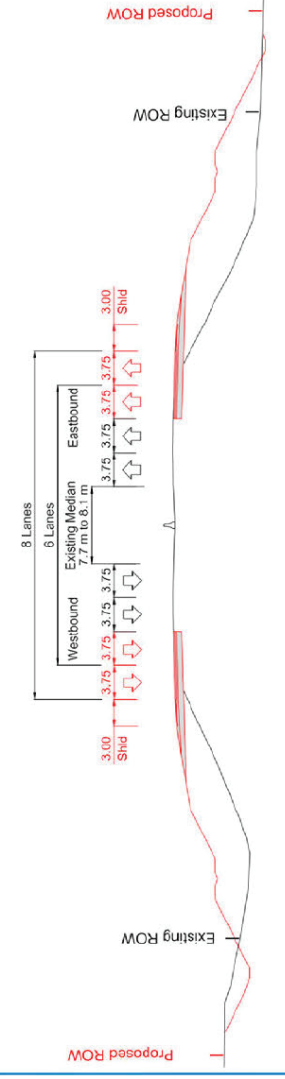
- Retains existing alignment of Sharpe's Lane, which minimizes environmental, property and utility impacts
- Maintains access across Highway 401 during construction
- Staged construction has fewer potential temporary impacts to local businesses compared to a full closure with detour

[Please click here to review a high-resolution version of Sharpe's Lane Alternatives](#)



Preferred Highway 401 Cross-Section

1 Maintain Existing Median
Widen to the Outside



2 Standard Median (7.5 m)
Widen to the Inside and the Outside



Alternative 1 (Widen to Outside), is preferred because:

- Lower cost compared to Alternative 2
- Minimizes impacts to traffic during construction
- Retains the existing median with concrete barrier

The proposed cross-section for this study will match the cross-sections of the previously completed Highway 401 studies adjacent to this study.



B Butler Creek Culvert

Culvert Improvement Alternatives



Replace with new culvert:

- + Long-term strategy
- + Accommodates interim (6-lane) Highway 401
- + Accommodates ultimate (8-lane) Highway 401
- Requires significant excavation



Culvert rehabilitation and retaining walls:

- Not a long-term strategy
- + Potential short-term strategy
- + Accommodates interim (6-lane) Highway 401
- Does not accommodate ultimate (8-lane) Highway 401
- Requires retaining walls

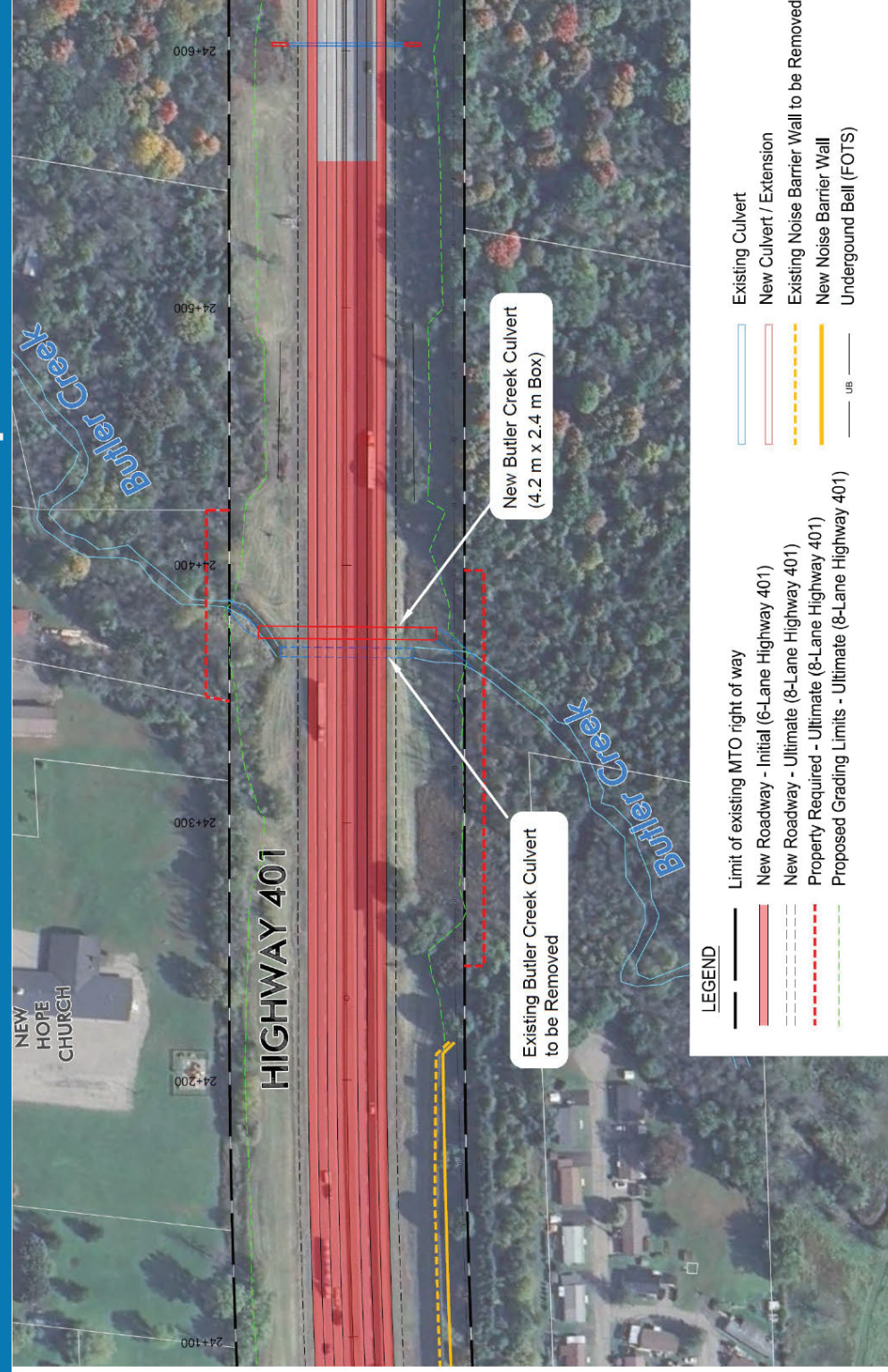
Culvert rehabilitation with extension:

- Not a long-term strategy
- + Potential short-term strategy
- + Accommodates interim (6-lane) Highway 401
- Does not accommodate ultimate (8-lane) Highway 401
- Future replacement of original culvert can be challenging



Butler Creek Culvert, Site 16X-0238/CO

Butler Creek Culvert Replacement



The Butler Creek Culvert Preferred Plan includes the existing culvert to be replaced with a new culvert at a new location east of the existing culvert. The Preferred Plan requires a minor realignment of Butler Creek, minor property impacts and significant excavation.

This alternative is preferred because:

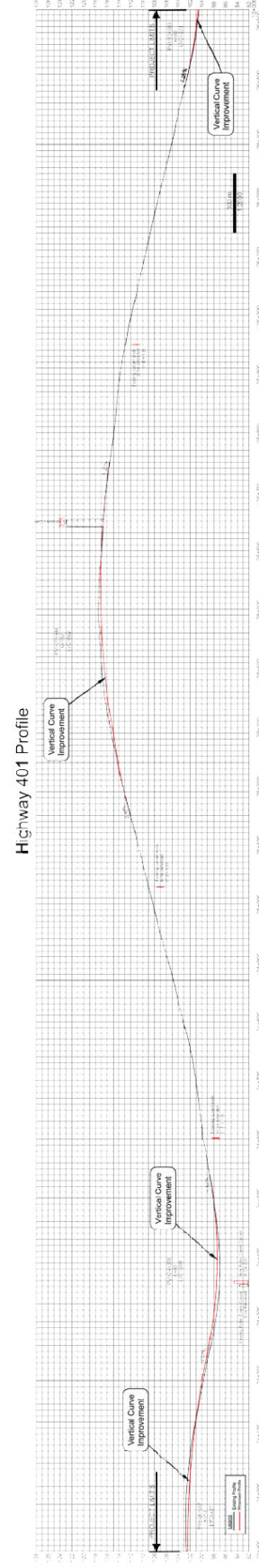
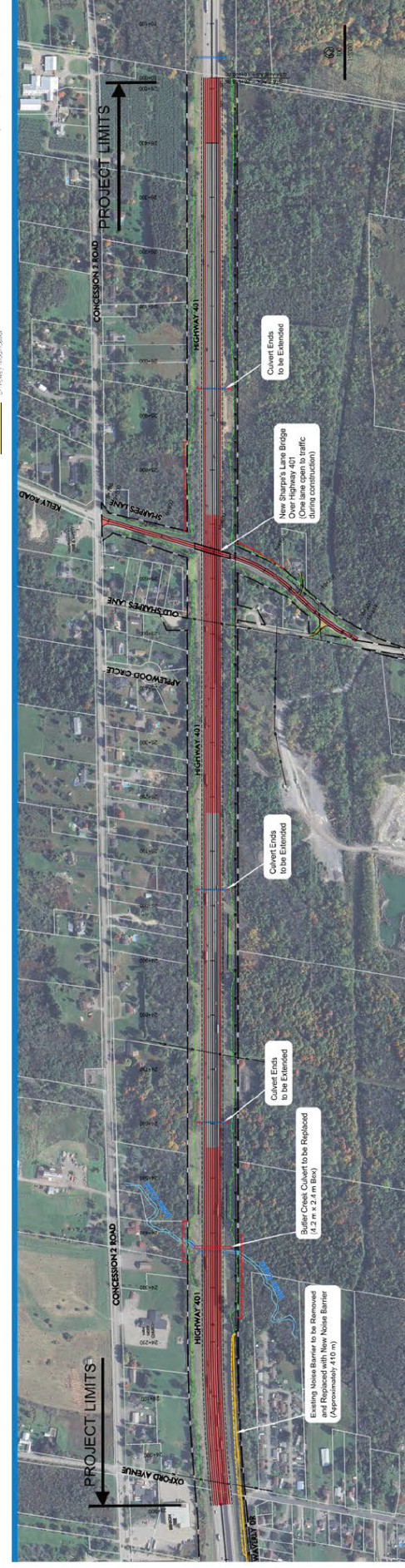
- The new culvert accommodates an interim 6-lane and ultimate 8-lane Highway 401 configuration
- Is a long-term strategy

Technically Preferred Plan

Highway 401 Technically Preferred Plan

Study Area Improvement Plan

- Existing Culvert
 - New Culvert Extension
 - Existing Noise Barrier Wall to be Retained
 - New Noise Barrier Wall
 - New Noise Barrier Wall
 - Compacted Fill and Gravel
 - Underground Box (if 273)
- LEGEND
- Right-of-Way (MTO) right of way
 - New Right-of-Way - Initial (6-Lane Highway 401)
 - New Right-of-Way - Ultimate (8-Lane Highway 401)
 - Property Boundary (1:10000) (Urban Highway 401)
 - Property Boundary (1:10000) (Rural Highway 401)
 - Drainage Interceptor

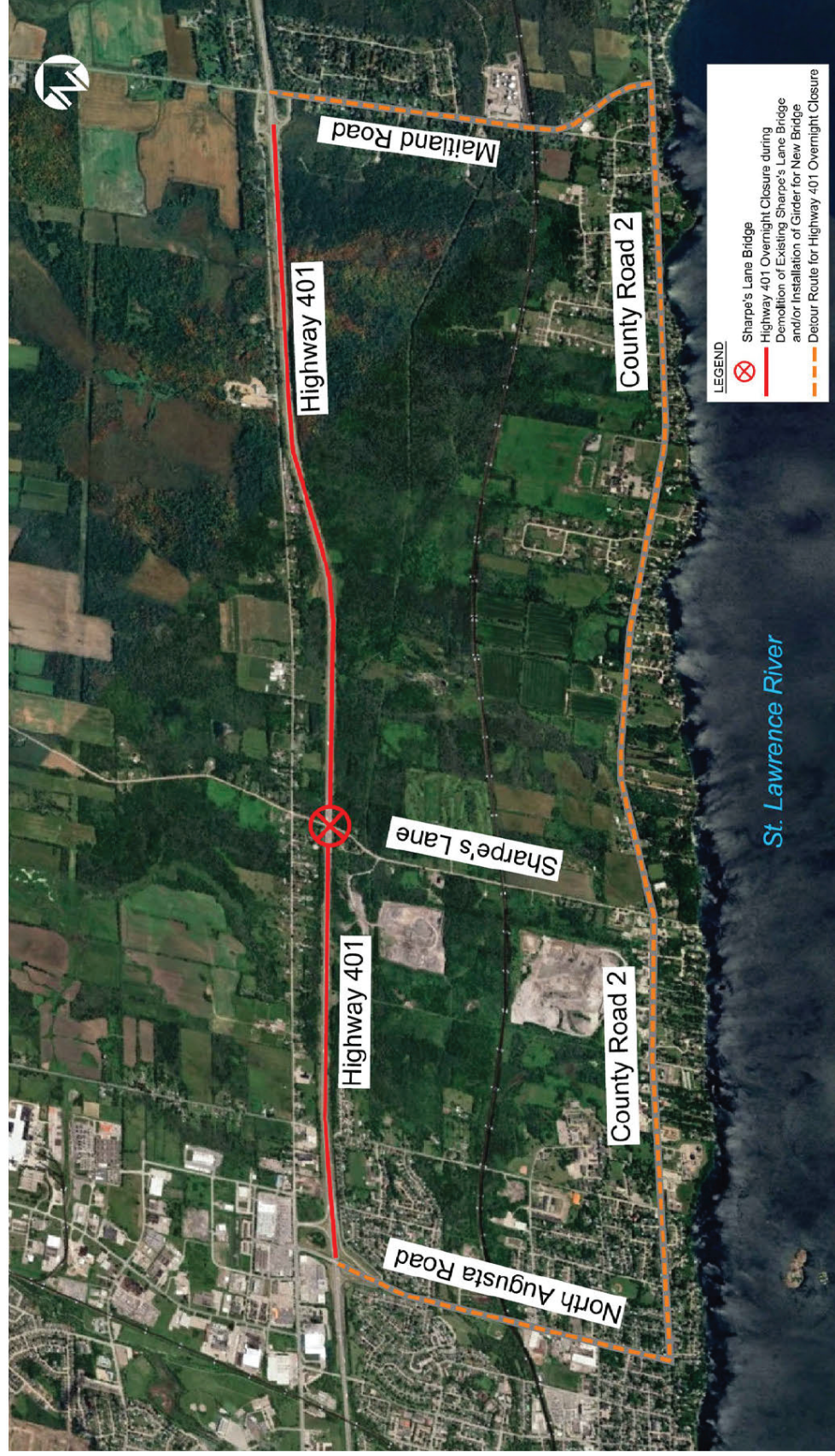


Highway 401 Planning Study, East of Brockville GWP 4111-22-00

Please [click here](#) to review a high-resolution version of this drawing



Construction Staging and Traffic Management



Highway 401 Full Closures / Potential Detour Route

- A detour is required for the removal of the existing Sharpe's Lane bridge over the highway and potential girder installation for the new bridge
- Traffic on Highway 401 to use adjacent interchanges at Maitland Road, North Augusta Road, and County Road 2
- Duration and number of closures will be confirmed during Detail Design
- The final detour routes will be confirmed in consultation with the affected municipalities



Designation and MTO Permit Control Areas

Once the Technically Preferred Alternative has been confirmed, the Ministry of Transportation may designate lands as highway and acquire property prior to EA process completion. A designation enables the Ministry to provide route/corridor protection through the application of development control measures along the corridor and in the vicinity of the designated lands. For more information about highway corridor management, please visit: <https://www.ontario.ca/page/highway-corridor-management>.

Example of MTO Permit Control Areas: Controlled-Access Highways (CAH)



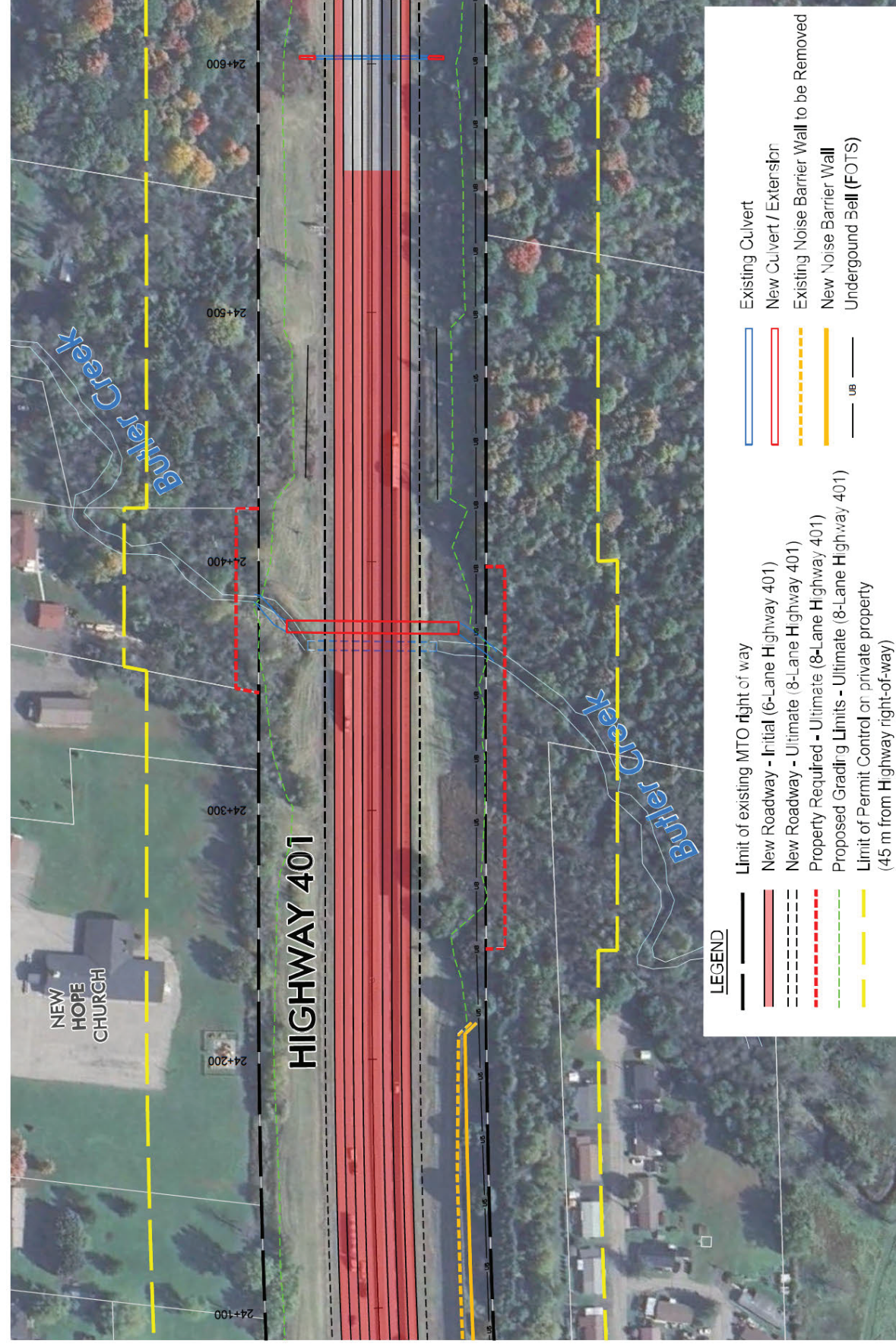
Any work on private property that is within 45 metres of the highway property or 395 metres from the centrepoint of an intersection or interchange requires approval from the MTO.

Any work on private property that is within 800 metres of the highway property and that may have a large impact on traffic, is also subject to approval from MTO.

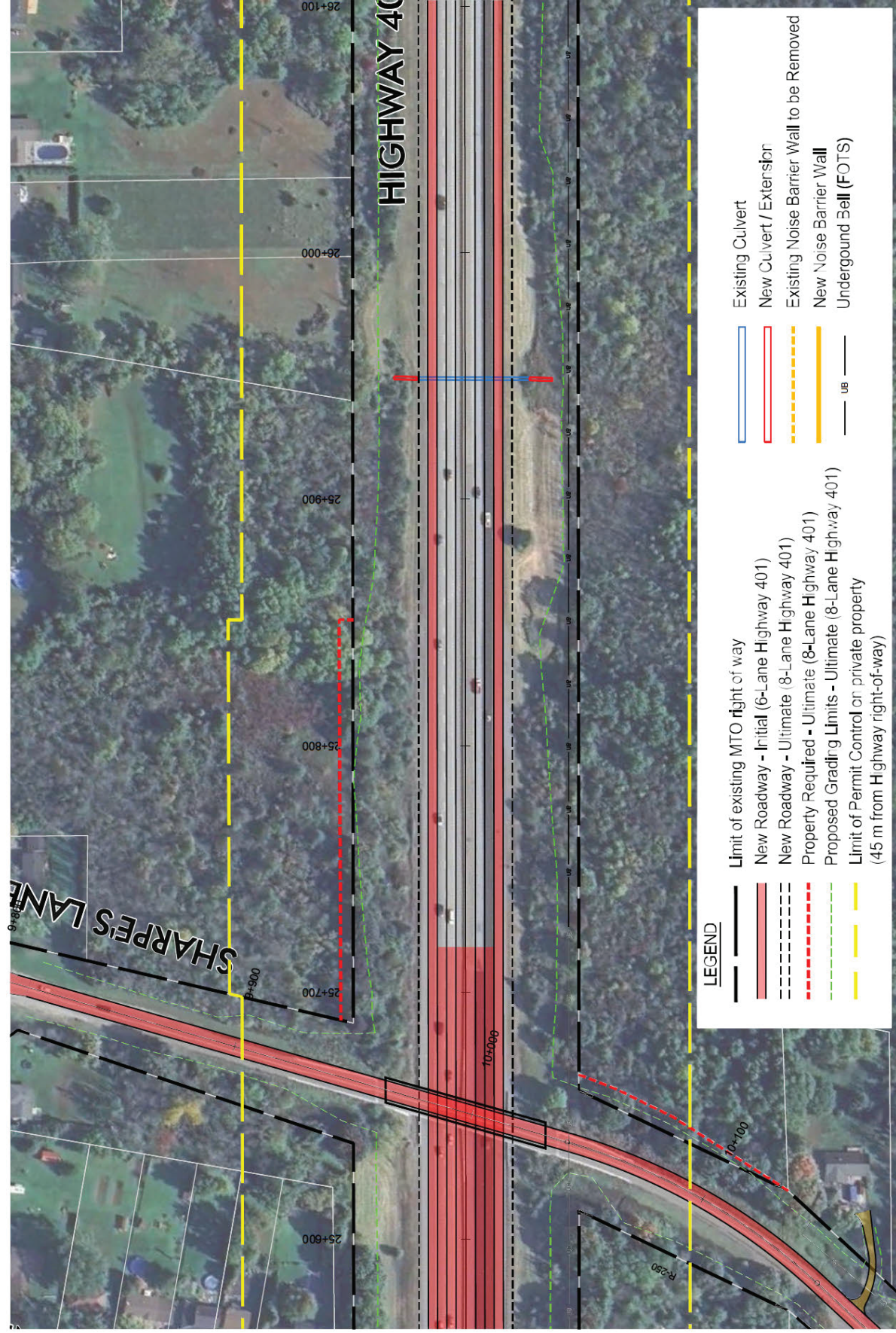
For more details on requirements for permits, please visit: <https://www.ontario.ca/page/highway-corridor-management> or make an inquiry regarding a specific property: <https://www.hcms.mto.gov.on.ca/>

Note: Limit of the MTO right-of-way shown on the figure is for illustrative purposes only.

New MTO Permit Control Areas

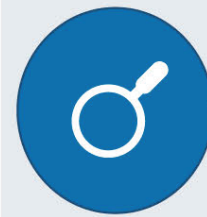


New MTO Permit Control Areas (continued)



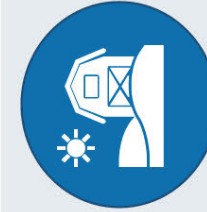
Potential Impacts and Proposed Mitigation Measures

Impacts resulting from this project will be minimized to the extent possible. Investigations are ongoing and will continue during Detail Design to help confirm environmental impacts, refine mitigation measures, and support obtaining required permits and approvals.



Archaeological & Built Heritage Resources

A Stage 1 Archaeological Assessment was completed and it identified areas with potential for archaeological resources. A Stage 2 Archaeological Assessment will be undertaken for areas identified as retaining archaeological potential during Detail Design.



Property

Potential impacts to private property are an important factor and component of the evaluation of alternatives. The project team will work to minimize impacts to private properties to the extent possible. Potentially affected property owners have been contacted directly by the project team.



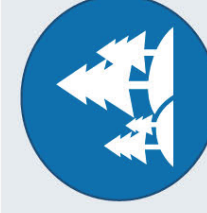
Noise Impacts

The potential changes in traffic noise associated with the Preferred Plan is currently being reviewed. A Noise Assessment is being completed in accordance with Provincial guidelines to determine if measures are required to mitigate potential increases in traffic noise (i.e., noise wall, landscaping, etc.). The results of the assessment will be documented in the TESR and made available for a comment period.



Traffic Operations

Construction activities are expected to impact traffic operations. Advanced notification will be provided to affected residents in advance of construction activities. Staging Plans will be finalized during detail design. Staging Plans will be finalized in consultation with municipalities and emergency service providers.



Natural Environment

Trees and vegetation will need to be removed and impacts will be minimized to the extent possible. A landscape design plan is being developed to identify opportunities for new plantings and landscape enhancements. Targeted species surveys will be completed during future studies for species that have the potential to be present. Mitigation measures to protect wildlife will be identified.

Next Steps

Following this Online Public Information Centre, the next steps in the Class EA process include:




- ✓ Review, consider and respond to comments received
- ✓ Confirm the Recommended Plan
- ✓ Confirm the potential impacts and proposed mitigation measures
- ✓ Prepare the Transportation Environmental Study Report (TESR)
- ✓ Issue Notice of Study Completion and file TESR for 30-day public comment period (Fall 2025)



[hwy401eastofbrockville.ca](http://www.hwy401eastofbrockville.ca)

Thank you for participating in this online PIC

Ways to provide your comments:

-  Complete the Online Comment Form
-  Email comments@hwy401eastofbrockville.ca
-  Contact by telephone:

Mr. Gregg Cooke, P.Eng.
Consultant Project Manager
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek ON L8J 0B4
Tel: 905-381-3227
comments@hwy401eastofbrockville.ca

Mr. David Brake
MTO Senior Project Manager
Ministry of Transportation
Asset Management – Capital Planning and Program Delivery
1355 John Counter Boulevard, Postal Bag 4000
Kingston ON K7L 5A3
Tel: 613-893-3031
Fax: 613-540-5106
comments@hwy401eastofbrockville.ca



Visit the Study website:
<http://www.hwy401eastofbrockville.ca>

We would appreciate receiving your comments by August 30, 2025



Freedom of Information and Protection of Privacy Act
Comments and information regarding this study are being collected to satisfy the requirements of the Ontario Environmental Assessment Act, and in accordance with the Freedom of Information and Privacy Act. With the exception of personal information, all comments will become part of the public record.

This study builds off of previously completed studies on Highway 401 adjacent to this study area and aligns with the future highway footprint identified as part of those adjacent studies.

Additional information for those studies is available at: www.highway401brockville.ca and www.highway401prescottmaitland.ca

Slide 4 (Class Environmental Assessment Process)

This study is being carried out under the requirements of the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024), which is approved under the Ontario Environmental Assessment Act for provincial transportation projects of a defined scope and magnitude.

The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process, which includes major improvements to existing provincial transportation facilities.

In 2019, modernization of the MTO Class EA (2000) was initiated to optimize the Class EA process by removing redundancies and expediting planning to facilitate a more streamlined implementation process. Amendments to the Class EA were approved in December 2023 and February 2024, and Class EA was renamed the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024). As part of the amendments, the EA process is now complete at the end of Preliminary Design, following the 30-day comment period for the Notice of Completion.

As part of the new MTO Class EA (2024), Preliminary Design is the primary opportunity to provide feedback on the project. Opportunities for meaningful consultation in future stages of the project will be minimal.

At the end of the study, a Transportation Environmental Study Report (TESR) will be prepared and made available for a 30-day comment period.

The goal of projects or activities covered under this Class E.A. is to provide a safe and effective transportation system while avoiding or minimizing negative environmental effects. The Class EA process consists of two main stages: Planning and Preliminary Design. The Planning stage focuses on developing a plan for how the project should be done, and the Preliminary Design stage focuses on 'roughing out' a design.

There are a number of steps to be completed as part of the Planning and Preliminary Design phases of the project, which allow for structured decision making to occur as the study progresses. These steps include:

- Data Collection – where available background information is reviewed, and field investigations are conducted as required to identify existing conditions in the study area
- Generating and evaluating alternatives – at this stage, Preliminary Design alternatives are developed to address structural needs, improve the highway safety and operations, and consider potential impacts to the existing natural, social, and cultural environment to identify a preferred plan
- Selection of a preferred plan – this step includes the identification of a preferred plan and proposes mitigation measures to address potential impacts to the environment
- Refine the preferred plan – as part of this step, the Preliminary Design of the preferred plan, including an implementation strategy, is proposed

- Reporting – once the information from the steps completed above have been gathered, considered, and finalized, the entire process is documented within a Transportation Environmental Study Report, or TESR, which will be posted on the project website for a 30-day public comment period. Notification of TESR Completion will be posted on the project website, within the *Brockville Recorder and Times* and *Brockville This Week* newspapers, and delivered via mail/email to those who have expressed an interest in this project
- Process Completion – this is an internal process that MTO uses to help to ensure that the Class EA requirements have been met before subsequent stages including detail design and construction begin

Consultation with the public, agencies, municipalities, and Indigenous communities, as well as identifying environmental protection measures are elements of the Planning and Preliminary Design phase of the project. Opportunities for consultation during this study are shown on this slide and include Public Information Centres, Municipal Technical Advisory Committee Meetings, responses to public notices, comments via the project website, and comments on the Transportation Environmental Study Report during the 30-day comment period.

The Implementation stage of the project, which includes Detail Design and construction of the proposed improvements, is not part of the Class E.A. process and will be undertaken in the future. As noted previously, opportunities for meaningful consultation during this stage of the project will be minimal as the Implementation stage is not part of the Class E.A. process. The timing of the Implementation stage of the project is not yet known.

Slide 5 (Online Public Information Centre 1 Summary)

The first online public information center was held between October 2, 2024 – November 8, 2024, to present and receive public input on the preliminary design alternatives and to address structural needs, improvements on highway safety and operations and consideration of potential impacts. Specifically, the PIC concentrated on the project background and the Class Environmental Assessment process, Transportation needs and opportunities in the study, preliminary structure alternatives for Sharpe's Lane Bridge, Highway 401 widening alternatives and preliminary existing study area conditions.

At and following the first P.I.C, we received comments on the following key themes:

- Potential impacts to property
- Potential impacts to safety and traffic
- Potential increase in noise
- Concerns with potential delays as a result of temporary closure of Sharpe's Lane Bridge during construction

Slide 6 (Project Overview)

As part of this study, bridge, drainage, and Highway 401 improvements are being considered. Sharpe's Lane Bridge is approaching the end of its service life and will need to be replaced. The bridge will need to accommodate the future Highway 401 footprints for the interim six and ultimate eight lanes.

In addition, the Butler Creek Culvert beneath Highway 401 is approaching the end of its service life and will need to be rehabilitated or replaced. The culvert will also need to accommodate the Highway 401 footprint for interim six and ultimate eight lanes.

Lastly, the study includes establishing the future Highway 401 footprints for the interim six lane and ultimate eight lane configurations.

Slide 7 (Problems and Opportunities)

The purpose of the study is to identify a recommended plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network. This study will include reviewing existing conditions, developing and evaluating alternatives, identifying a preferred plan, and developing environmental protection/mitigation measures.

At the completion of the study, a Recommended Plan for Highway 401 will be confirmed and designated (i.e., protected). The Ministry will be registering a designation of a proposed highway plan based on the future needs determined by this Planning and Preliminary Design study. A designation is a mechanism for the Ministry to provide route/corridor protection as well as impose development control measures along the corridor and on adjoining owners. This type of designation is most often used where lands are still under private ownership, sometimes years in advance of intended property acquisition and construction. The study has been initiated to address the following problems and opportunities:

Problems

- The bridge and structural culvert in the Study Area are nearing the end of their service life and will require rehabilitation and/or replacement in the near future.
- The existing Highway 401 platform cannot accommodate the traffic staging required to rehabilitate or replace the bridge and structural culvert

Opportunities

- Develop appropriate rehabilitation or replacement strategies to maintain the safe operation of the highway corridor for the current and future planning horizons
- Identify the ultimate footprint for the Highway 401 corridor to address current and future transportation needs

Slide 8 (Investigations)

In accordance with the MTO class environmental assessment process, this study includes engineering and environmental specialists who have carried out/are carrying out background studies and site-specific investigations to support the development and the evaluation of alternatives and identify potential impacts and mitigation measures. The investigations for this study include, but are not limited to, the following:

- Engineering investigations include highway, traffic and safety, bridge, drainage and hydrology, geotechnical, foundations and electrical.
- Natural Environment investigations include terrestrial ecosystems, fish and fish habitat and species at risk.

- Socio-Economic investigations include cultural and built heritage, noise and air quality, groundwater, archaeology and contamination.

Slide 9 (Evaluation of Alternatives)

The evaluation process includes identifying evaluation criteria through the input received during this study, the project team's experience in projects of this nature, provincial guidelines, and existing study area conditions.

As presented at P.I.C. 1, the Alternatives were assessed using the factors and criteria presented in this slide, including potential impacts and criteria pertaining to transportation and engineering, and socio-economic, natural, and cultural environments.

Comments received from agencies, stakeholders, Indigenous communities, and members of the public were incorporated into the evaluation.

The Technically Preferred Plan is selected as the aggregate of Preliminary Design alternatives that achieve the best overall balance of transportation engineering, individual environmental factor impacts, and overall environmental impact, taking into consideration the net environmental effects by applying conceptual mitigation measures.

Slide 10 (Evaluation of Structure Alternatives Sharpe's Lane)

As noted, this study includes the evaluation of the design modifications to Sharpe's Lane Bridge. A range of alternatives for replacing this bridge have been developed, including replacement of the bridge on its existing alignment with either the bridge closed with a detour or the bridge open with a single lane of traffic, on a new alignment to the west, and on a new alignment to the east.

Based on the Highway Engineering Evaluation Criteria, Alternative 1B was found to be moderately preferred because it:

- Retains the existing alignment of Sharpe's Lane and
- Maintains access across Highway 401 during construction

Based on the Social and Cultural Environment Evaluation Criteria, Alternative 1B was found to be the most preferred because it:

- Requires less property compared to Alternative 2 and 3
- Results in fewer potential temporary impacts to local businesses compared to Alternative 1A (does not require detour)
- Results in fewer impacts to areas of archaeological, built heritage and contamination potential compared to Alternatives 2 and 3

Based on the Natural Environment Evaluation Criteria, Alternative 1B was most preferred because it has:

- Fewer potential impacts to forested areas compared to Alternatives 2 and 3

Based on the evaluation, replacement of the bridge on its existing alignment with the bridge open with a single lane (Alternative 1B) was selected as most preferred.

Slide 11 (Preferred Sharpe's Lane Bridge)

The preferred alternative for Sharpe's Lane Bridge is Alternative 1B because it:

- Retains existing alignment of Sharpe's Lane, which minimizes environmental, property and utility impacts
- Maintains access across Highway 401 during construction
- Has fewer potential temporary impacts to local businesses compared to a full closure with detour

Slide 12 (Preferred Highway 401 Cross-Section)

Two alternatives for improvements to Highway 401 were developed. Alternative 1 includes maintaining the existing median and widening the highway to the outside, and Alternative 2 includes a standard median and widening the highway to both the inside and outside. The preliminary screening determined that only Alternative 1 should be carried forward as it retains the existing median and concrete barrier, minimizes impacts to traffic during construction, and has a lower cost compared to Alternative 2. Alternative 2 was screened out as it requires more complicated traffic staging compared to Alternative 1, requires reconstruction of the median including concrete barrier, and has a higher cost compared to Alternative 1.

Alternative 1 has been selected as the preferred option because it:

- Has a lower cost compared to Alternative 2
- Minimizes impacts to traffic during construction
- Retains the existing median with concrete barrier
- Is consistent with the proposed cross section of the previously completed Highway 401 studies on either side of the study limits.

Slide 13 (Drainage Improvements Strategy)

Butler Creek Culvert is the only structural culvert in the Study Area. Three improvement strategies were considered for the existing culvert, including replacement with a new culvert, culvert rehabilitation and construction of retaining walls, and culvert rehabilitation with a culvert extension.

Replacement of the existing culvert with a new culvert is the preferred alternative as it will accommodate both the interim 6-lane and ultimate 8-lane Highway 401 configuration but requires significant excavation.

Slide 14 (Butler Creek Culvert Replacement)

The Butler Creek Replacement of the existing culvert with a new culvert alternative is preferred because it:

- Retains the existing alignment of Sharpe's Lane, which minimizes environmental, property and utility impacts
- Maintains access across Highway 401 during construction
- Staged construction has fewer potential temporary impacts to local businesses compared to a full closure with detour

Slide 15 (Technically Preferred Plan)

The Technically Preferred Plan includes:

- Replacement of the existing Sharpe's Lane bridge over Highway 401
- Replacement of the existing Butler Creek culvert
- Widening of Highway 401 to an interim 6-lane and ultimate 8-lane cross-section
- Drainage improvements
- Replacement of existing noise barrier wall along Highway 401

Slide 16 Construction Staging and Traffic Management

Highway 401 Full Closures / Potential Detour Route:

A detour is required for the removal of the existing Sharpe's Lane bridge over the highway and potential girder installation for the new bridge. Traffic on Highway 401 will use adjacent interchanges at Maitland Road, North Augusta Road, and County Road 2 during the closures. The duration and number of closures will be confirmed during Detail Design. The final detour routes will be confirmed in consultation with the affected municipalities

Slide 17 Designation and MTO Permit Control Areas

Once the Technically Preferred Alternative has been confirmed, the Ministry of Transportation may designate lands as highway and acquire property prior to EA process completion. A designation enables the Ministry to provide route/corridor protection through the application of development control measures along the corridor and in the vicinity of the designated lands.

For more information about highway corridor management, please visit:
<https://www.ontario.ca/page/highway-corridor-management>.

Any work on private property that is within 45 metres of the highway property or 395 meters from the centrepoint of an intersection or interchange requires approval from the MTO.

Any work on private property that is within 800 metres of the highway property and that may have a large impact on traffic, is also subject to approval from MTO.

For more details on requirements for permits, please visit:
<https://www.ontario.ca/page/highway-corridor-management> or make an inquiry regarding a specific property: <https://www.hcms.mto.gov.on.ca/>

Note: Limit of the MTO right-of-way shown on the figure is for illustrative purposes only.

Slide 18 New MTO Permit Control Area 1:

Changes to the existing MTO Permit Control Area along Highway 401 will be required to accommodate the Preferred Plan at the Butler Creek Culvert, which include minor impacts to private properties, as shown on the figure presented.

Slide 19 New MTO Permit Control Area 2:

Changes to the existing MTO Permit Control Area along Highway 401 will be required to accommodate the Preferred Plan at Sharpe's Lane, which include minor impacts to private properties, as shown on the figure presented.

Slide 20 (Potential Impacts and Proposed Mitigation Measures)

Impacts resulting from this project will be minimized to the extent possible. Preliminary mitigation measures have been identified in this slide and will be further refined during Detail Design. Investigations are ongoing and will continue to help confirm environmental impacts, refine mitigation measures, and support obtaining required permits and approvals.

With regard to the Archeological Resources a Stage 1 Archeological Assessment was completed and it identified areas with potential for archaeological resources. A Stage 2 Archeological Assessment will be undertaken during Detail Design for areas identified as retaining archaeological potential.

Potential impacts to private property are an important factor and component of the evaluation of alternatives. The project team will work to minimize impacts to private properties to the extent possible.

Potentially affected property owners have been contacted directly by the project team. With regard to noise impacts, the potential changes in traffic noise associated with the Preferred Plan is currently being reviewed. A Noise Assessment is being completed in accordance with Provincial guidelines to determine if measures are required to mitigate potential increases in traffic noise (i.e., noise wall, landscaping, etc.) and any required mitigation will be included in the final Recommended Plan.

Construction activities are expected to impact traffic operations. Advanced notification will be provided to affected residents in advance of construction activities. Staging Plans will be finalized during detail design. Staging Plans will be finalized in consultation with municipalities and emergency service providers.

No significant fish habitat was identified in the study area as part of the Fish and Fish Habitat Existing Conditions Report. Potential impacts to fish and fish habitat may occur as a result of the Preferred Plan (including the replacement of the Butler Creek Culvert). Appropriate protection measures including in-water timing windows (i.e., no in-water work from March 15 to June 30) will be proposed. Detailed protection measures will be documented in the Transportation Environmental Study Report. Lastly, vegetation removal will be minimized to the extent possible. A landscape design plan is being developed to identify opportunities for new plantings and landscape enhancements. Targeted species surveys will be completed during future studies for sensitive species that have the potential to be present. Protection and mitigation measures to protect wildlife will be identified and documented in the Transportation Environmental Study Report.

Slide 21 (Next Steps)

The Project Team will complete the following after this Public Information Centre:

- Review, consider, and respond to the comments received
- Confirm the Recommended Plan
- Confirm the potential impacts and proposed mitigation measures
- Prepare the Transportation Environmental Study Report for the project
- Issue a Notice of Study Completion and file the Transportation Environmental Study Report for a 30-day comment period, which is tentatively scheduled for Fall 2025

Slide 22 (Thank you)

On behalf of the Ontario Ministry of Transportation, we would like to thank you for your interest in this study, and for taking the time to participate in this online Public Information Centre. Your input is very important to us, and there are several ways to provide your comments.

We encourage you to review all PIC materials available on the project website, as well as to submit comments or questions to the project team.

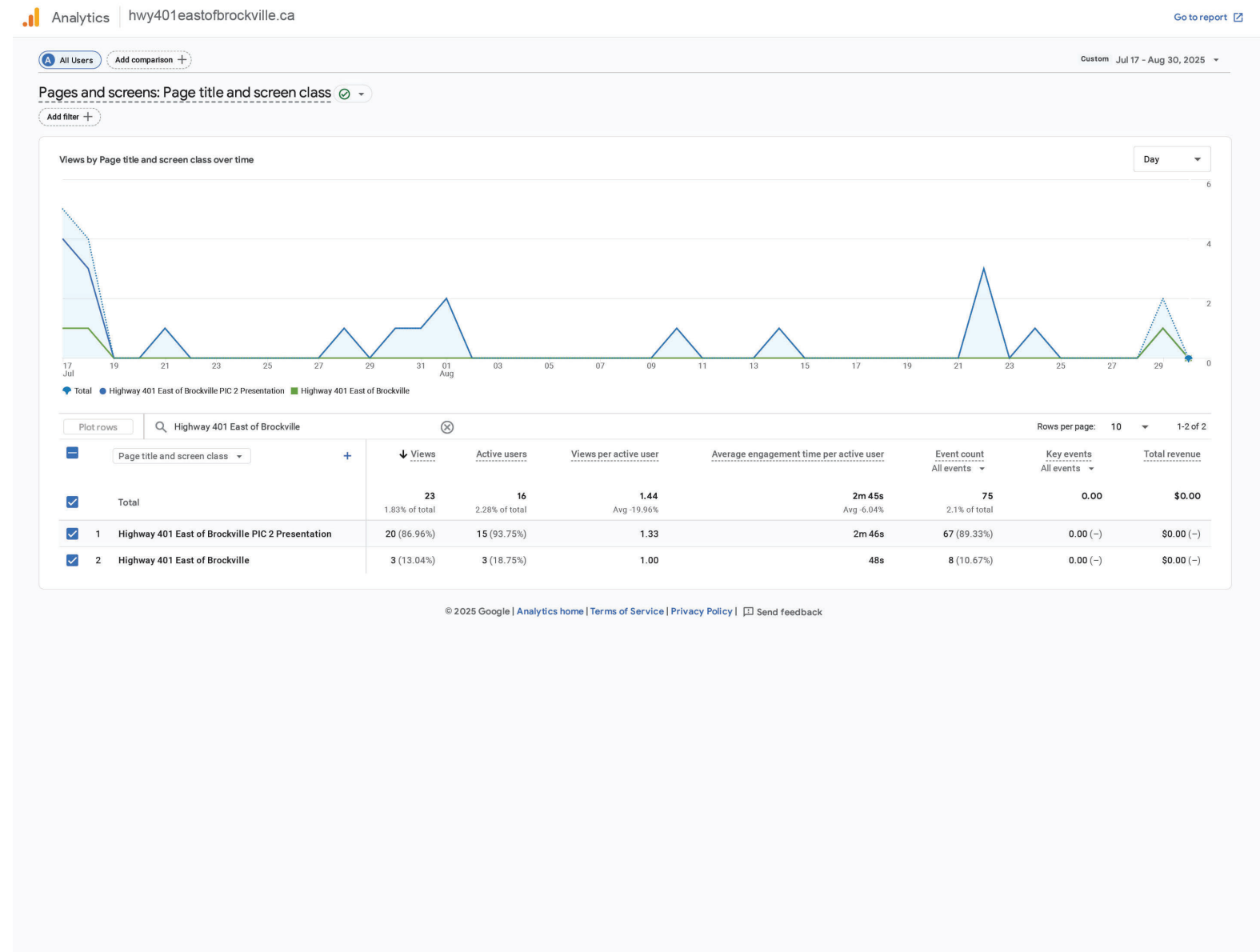
If you have any comments or questions, please do not hesitate to contact the project team via email at: comments@hwy401eastofbrockville.ca, or by contacting one of the project team members listed on this slide by telephone.

We would appreciate receiving any comments or questions you may have by August 30, 2025. Thank you again for taking the time to participate in this online PIC.

**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 2**

0 APPENDIX C
Online Participation Statistics
September 15, 2025

APPENDIX C Online Participation Statistics



APPENDIX D Comments Received



From: Brake, David (MTO) <Dave.Brake@ontario.ca>
Sent: Tuesday, July 29, 2025 11:41 AM
To: Cooke, Gregg <gregg.cooke@stantec.com>; Gazibara, Nevena <Nevena.Gazibara@stantec.com>
Cc: Dickson, Amanda (MTO) <Amanda.Dickson@Ontario.ca>; Belliveau, Tim <tim.belliveau@stantec.com>; Adams, John (MTO) <John.Adams@ontario.ca>
Subject: FW: From Yuan & Li: Property Impact Pin#44160061 - Highway 401 Project east of Brockville

I spoke with [REDACTED] this morning; he had left me several voicemails last week.

He is open to the property acquisition process, but I think he thought he could leverage this acquisition to gain access to the back of his property from the highway. I let him know that the 401 is a controlled access highway and access is limited to interchanges, so access to his property would not be approved. He wants to retire to the area and maybe develop a small hobby farm, he purchased the land about four years ago.

He is happy to help where he can and wants to meet with corridor to discuss what options he has for access. I will send them a separate e-mail in this regard.

I let him know that this is a planning study and that we would not be looking to acquire the property for at least five years.

Dave

David Brake

Senior Project Manager | Capital Planning & Program Development
Asset Management Branch | TIMD
Ministry of Transportation | Ontario Public Service
613-893-3031 | dave.brake@ontario.ca



Taking pride in strengthening Ontario, its places and its people

Upcoming holidays August 4th – 8th

From: [redacted]
Sent: Sunday, July 6, 2025 2:34 PM
To: Brake, David (MTO) <Dave.Brake@ontario.ca>
Cc: Gregg.Cooke@stantec.com
Subject: From Yuan & Li: Property Impact Pin#44160061 - Highway 401 Project east of Brockville

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi David/Gregg

We have just picked up the registered mail, and thank you for informing us the impact on our property.

I am studying it and I think it is a great project. If the project needs to access or expand into our land, please contact me by my cell phone at [redacted] and leave message. Or you can email me to [redacted]

I am recovering from a Hwy401 car accident, and I could sometimes reply a little bit late.

[redacted]

Caution: This email originated from outside of Stantec. Please take extra precaution.

Attention: Ce courriel provient de l'extérieur de Stantec. Veuillez prendre des précautions supplémentaires.

Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.

From: [redacted]

Sent: Monday, August 11, 2025 12:13 PM

To: Cooke, Gregg <gregg.cooke@stantec.com>

Subject: Voice Mail (1 minute and 16 seconds)

hi [redacted] my name is jennifer larley i live on [redacted] umm regarding the notice of public information preliminary design and class environmental assessment of hwy 4 O1 planning study east of brockville and we just wanted a bit more information the picture there is a bit difficult to read in terms of specifically where the the limitation of the line would be we're fairly close and we saw someone measure some things and we received the letter so we just want to make sure how this will affect us and if this is actually going to to happen and and how many years approximately and a few more information but if you could give us a call back uh you can reach me at [redacted] thank you very much looking forward to hear from you have a good day bye bye

You received a voice mail [redacted]

Thank you for using Transcription! If you don't see a transcript above, it's because the audio quality was not clear enough to transcribe.

[Set Up Voice Mail](#)

Caution: This email originated from outside of Stantec. Please take extra precaution.

Attention: Ce courriel provient de l'extérieur de Stantec. Veuillez prendre des précautions

Nevena Gazibara, B.Sc., MREM, Env. SP.
Environmental Planning Team Lead
She, Her, Hers

-----Original Message-----

From: [REDACTED]
Sent: Thursday, July 17, 2025 9:03 AM
To: comments@hwy401eastofbrockville.ca
Subject: Noise barrier

[You don't often get email [REDACTED]. Learn why this is important at <https://aka.ms/LearnAboutSenderIdentification>]

I do hope there is money in your budget for a noise barrier from Sharps lane to just past Applewood Circle. Our houses are soo close to the 401 we have to shout at each other while in the garden. Concerned there will be more noise because of the expansion and our property value will be much reduced.

Sent from my iPhone

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Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.

Nevena Gazibara, B.Sc., MREM, Env. SP.
Environmental Planning Team Lead
She, Her, Hers

-----Original Message-----

From: [REDACTED]
Sent: Monday, July 21, 2025 2:15 PM
To: comments@hwy401eastofbrockville.ca
Subject: Noise pollution

[You don't often get email [REDACTED] why this is important at <https://aka.ms/LearnAboutSenderIdentification>]

For David Brake, Senior project Manager, _____

Please make allowances in your budget for a noise barrier for Old Sharpes Lane and Cottonwood Circle. I don't think a survey was done ever and if 6 lanes or 8 lanes of 401 is built our property value will plummet. We really need it. Thanks

Sent from my iPhone

Caution: This email originated from outside of Stantec. Please take extra precaution.

Attention: Ce courriel provient de l'extérieur de Stantec. Veuillez prendre des précautions supplémentaires.

Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.

Nevena Gazibara, B.Sc., MREM, Env. SP.
Environmental Planning Team Lead
She, Her, Hers

-----Original Message-----

From: [REDACTED]
Sent: Friday, August 1, 2025 10:42 AM
To: comments@hwy401eastofbrockville.ca
Subject: 90 Manor Drive

You don't often get email from [REDACTED]. [Learn why this is important](https://aka.ms/LearnAboutSenderIdentification)
<<https://aka.ms/LearnAboutSenderIdentification>>

Good morning:

I am trying to find more information about why I received the notice of public information centre 2. I am in the process of putting significant money into my home in preparation for retirement and need to understand how highway improvements may impact my property. If there is a possibility the MTO will require my land, I would like to know so that I can make decisions regarding my future.

Could someone please contact me about this?

Thank you

[REDACTED]

Caution: This email originated from outside of Stantec. Please take extra precaution.

Attention: Ce courriel provient de l'extérieur de Stantec. Veuillez prendre des précautions supplémentaires.

Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.



From: [REDACTED]
Sent: Tuesday, July 22, 2025 9:07 AM
To: comments@hwy401eastofbrockville.ca
Subject: GWP 4111-22-00

You don't often get email from richjoudoin@gmail.com. [Learn why this is important](#)

Hello,

I am a resident at [REDACTED] - despite the "Brockville" assignment, I am the first house south of the Maitland 401 Eastbound exit. See the red dot for reference:

HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 2

0 APPENDIX E
Indigenous Engagement
September 15, 2025



I just received another notice regarding Project GWP 4111-22-00. When looking at the included map, it seems that my house would be well short of the impact area. Regardless, I have heard and read multiple different accounts of property acquisitions as part of the interim six lane and ultimate eight lane highway in this area. If my property will indeed be impacted by this project, can someone give me some sort of timeline as to when it may have to be purchased, if at all?

Regards,

[Redacted]

Caution: This email originated from outside of Stantec. Please take extra precaution.

Attention: Ce courriel provient de l'extérieur de Stantec. Veuillez prendre des précautions supplémentaires.

Atención: Este correo electrónico proviene de fuera de Stantec. Por favor, tome precauciones adicionales.



APPENDIX E Indigenous Engagement

From: [Sacchetti, Franca \(She/Her\) \(MTO\)](#)
To: [Grand Chief](#)
Cc: [Adam Jacobs](#); [Cactus Sunday](#); [Kayla Sunday](#); [Copping, Peter A. \(MTO\)](#)
Subject: Notice of Online Public Information Centre 2 - GWP 4111-22-00
Date: Wednesday, July 2, 2025 9:50:05 AM
Attachments: [image001.png](#)
[QGN_PIC2_PrelimDesign_401_Mallorytown-Brockville_GWP4111-22-00.pdf](#)
[PIC2_PrelimDesign_401_Mallorytown-Brockville_GWP4111-22-00_MoA.pdf](#)

Dear Grand Chief Akwiratekha Lazore,

Please see the attached notice of online public information regarding the Highway 401 Planning Study East of Brockville.

Should you have any questions or concerns, please feel free to reach out.

Sincerely,

Franca Sacchetti

Regional Director, East Region
Operations Division
Ministry of Transportation | Ontario Public Service
647-638-5359, franca.sacchetti@ontario.ca



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Ministry of Transportation

Regional Services & Relationships
East Operations Branch
1355 John Counter Boulevard
Postal Bag 4000
Kingston, Ontario K7L 5A3
Tel.: 613-545-4600
Fax: 613-547-1777

Ministère des Transports

Services Régionales & Relations
Opérations de l'Est
1355 Boulevard John Counter
CP/Service de sacs 4000
Kingston (Ontario) K7L 5A3
Tél.: 613-545-4600
Télééc. 613-547-1777



July 2, 2025

Mohawks of Akwesasne
CIA III 101 Tewesateni Rd., Kawehno:ke
Akwesasne ON K6H 5R7

Attention: Grand Chief Abram Benedict

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 2**

Dear Grand Chief:

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of the study is to identify a Recommended Plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network.

This preliminary design study is following an approved planning process under the *Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024)* for a Group "B" project and will document the existing conditions and sensitivities of the study area, examine potential impacts/changes, provide recommendations for proposed mitigation, and outline any commitments for future environmental work. The first online Public Information Centre (PIC) was held from October 2, 2024 to November 8, 2024, to present and solicit feedback on the study background, existing study area conditions, preliminary improvement alternatives, the evaluation process, and next steps in the Class EA process. As part of this Class EA Study, the project team has undertaken field investigations and technical analysis to identify and evaluate preliminary design alternatives related to the above study objectives.

July 2, 2025
Page 2 of 2

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 2**

This study includes reviewing existing conditions, developing and evaluating alternatives, identifying appropriate improvements, and developing environmental protection and mitigation measures. A Recommended Plan will be confirmed and designated (protected) at the completion of the study. A *Transportation Environmental Study Report* will be prepared documenting the existing natural and socio-economic environment, a summary of the Recommended Plan and design features, potential impacts of the undertaking, required mitigation procedures, and commitments to future work.

The purpose of this letter is to advise you that the second online Public Information Centre (PIC) will be available for your review on the study website, www.hwy401eastofbrockville.ca, starting on **July 17, 2025**. A recorded presentation will be available as part of the online PIC.

The purpose of this online PIC 2 is to present and seek input on evaluation of alternatives, the technically preferred plan, and next steps in the Class EA process. The project website will also be regularly updated with current project information and can be used as an additional resource to submit comments to the project team.

If you have any questions or comments, or if you require further information regarding this project, please feel free to contact me, Franca Sacchetti, Director of East Operations at franca.sacchetti@ontario.ca, or Peter Copping, Indigenous Liaison Specialist at peter.a.copping@ontario.ca. The MTO will continue to engage with you as this project progresses.

Sincerely,

Franca Sacchetti
Director

c. D. Brake, P. Copping – Ministry of Transportation
G. Cooke, N. Gazibara – Stantec Consulting Ltd.
A. Francis, C. Francis, C. Sunday, K. Sunday – Mohawks of Akwesasne

Attachment: Notice of Public Information Centre 2

From: [Sacchetti, Franca \(She/Her\) \(MTO\)](#)
To: [Consultations](#)
Cc: [Copping, Peter A. \(MTO\)](#)
Subject: Notice of Online Public Information Centre 2 - GWP 4111-22-00
Date: Wednesday, July 2, 2025 9:53:34 AM
Attachments: [image001.png](#)
[OGN_PIC2_PrelimDesign_401_Mallorytown-Brockville_GWP4111-22-00.pdf](#)
[PIC2_PrelimDesign_401_Mallorytown-Brockville_GWP4111-22-00_MNO.pdf](#)

Dear Sir/Madam,

Please see the attached notice of online public information regarding the Highway 401 Planning Study, East of Brockville.

Should you have any questions or concerns, please feel free to reach out.

Sincerely,

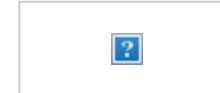
Franca Sacchetti

Regional Director, East Region

Operations Division

Ministry of Transportation | Ontario Public Service

647-638-5359, franca.sacchetti@ontario.ca



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Télééc. 613-547-1777



July 2, 2025
Page 2 of 2

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 2**

This study includes reviewing existing conditions, developing and evaluating alternatives, identifying appropriate improvements, and developing environmental protection and mitigation measures. A Recommended Plan will be confirmed and designated (protected) at the completion of the study. A *Transportation Environmental Study Report* will be prepared documenting the existing natural and socio-economic environment, a summary of the Recommended Plan and design features, potential impacts of the undertaking, required mitigation procedures, and commitments to future work.

The purpose of this letter is to advise you that the second online Public Information Centre (PIC) will be available for your review on the study website, www.hw401eastofbrockville.ca, starting on **July 17, 2025**. A recorded presentation will be available as part of the online PIC.

The purpose of this online PIC 2 is to present and seek input on evaluation of alternatives, the technically preferred plan, and next steps in the Class EA process. The project website will also be regularly updated with current project information and can be used as an additional resource to submit comments to the project team.

If you have any questions or comments, or if you require further information regarding this project, please feel free to contact me, Franca Sacchetti, Director of East Operations at franca.sacchetti@ontario.ca, or Peter Copping, Indigenous Liaison Specialist at peter.a.copping@ontario.ca. The MTO will continue to engage with you as this project progresses.

Sincerely,

Franca Sacchetti
Director

c. D. Brake, P. Copping – Ministry of Transportation
G. Cooke, N. Gazibara – Stantec Consulting Ltd.

Attachment: Notice of Public Information Centre 2

July 2, 2025

Métis Nation of Ontario
Suite 1100 - 66 Slater Street
Ottawa ON K1P 5H1

Attention: Consultation Coordinator

**Reference: Highway 401 Planning Study East of Brockville
GWP 4111-22-00 - Notice of Online Public Information Centre 2**

Dear Sir / Madam:

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Preliminary Design and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville, in the United Counties of Leeds and Grenville.

The purpose of the study is to identify a Recommended Plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network.

This preliminary design study is following an approved planning process under the *Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways (2024)* for a Group "B" project and will document the existing conditions and sensitivities of the study area, examine potential impacts/changes, provide recommendations for proposed mitigation, and outline any commitments for future environmental work. The first online Public Information Centre (PIC) was held from October 2, 2024 to November 8, 2024, to present and solicit feedback on the study background, existing study area conditions, preliminary improvement alternatives, the evaluation process, and next steps in the Class EA process. As part of this Class EA Study, the project team has undertaken field investigations and technical analysis to identify and evaluate preliminary design alternatives related to the above study objectives.

**HIGHWAY 401 PLANNING STUDY EAST OF BROCKVILLE
ONLINE PUBLIC INFORMATION CENTRE 2**

0 APPENDIX F
Newspaper Articles
September 15, 2025

**APPENDIX F
Newspaper Articles**

Notice of Public Information Centre 2

**Preliminary Design and Class Environmental Assessment
Highway 401 Planning Study East of Brockville
(GWP 4111-22-00)**

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake a Preliminary Design, and Class Environmental Assessment (Class EA) Study on Highway 401 for the replacement and rehabilitation of one bridge and one culvert, and identifying the future Highway 401 footprint for an interim six lanes and ultimate eight lanes, from 0.75 km east of North Augusta Road to 3.3 km west of Maitland Road (approximately 2.6 km), in the Township of Elizabethtown-Kitley, east of Brockville. The purpose of the study is to identify a Recommended Plan that addresses current and future transportation needs in the study area as part of the Ministry's ongoing review of safety and operational needs for the provincial highway network.

THE PROCESS

This study is following the approved planning process for a Group 'B' project under the Class Environmental Assessment for Provincial Transportation Facilities and Municipal Expressways, 2024 (Class EA). The project team, which includes representatives from Stantec and MTO, have completed environmental and engineering field investigations and are seeking input from individuals, organizations, and Indigenous communities. Upon completion of Preliminary Design, the project team will prepare a Transportation Environmental Study Report (TESR) and issue a Notice of Completion. The project team will include comments received, responses to the comments and any supporting materials in a record of consultation.

Once a Technically Preferred Alternative has been identified, the Ministry of Transportation may designate lands as highway and acquire property prior to EA process completion. A designation enables the Ministry to provide route/corridor protection through the application of development control measures along the corridor and in the vicinity of the designated lands. For more information about highway corridor management, please visit: <https://www.ontario.ca/page/highway-corridor-management>.

ONLINE PUBLIC INFORMATION CENTRE 2

Public Information Centre (PIC) 1 was offered on the study website between October 2, 2024, and November 8, 2024, to present and solicit feedback on the study background, existing study area conditions, preliminary improvement alternatives, the evaluation process, and next steps in the Class EA process.

PIC 2 is currently being planned to present and gather feedback on the evaluation of alternatives, the technically preferred plan, and next steps in the Class EA process.

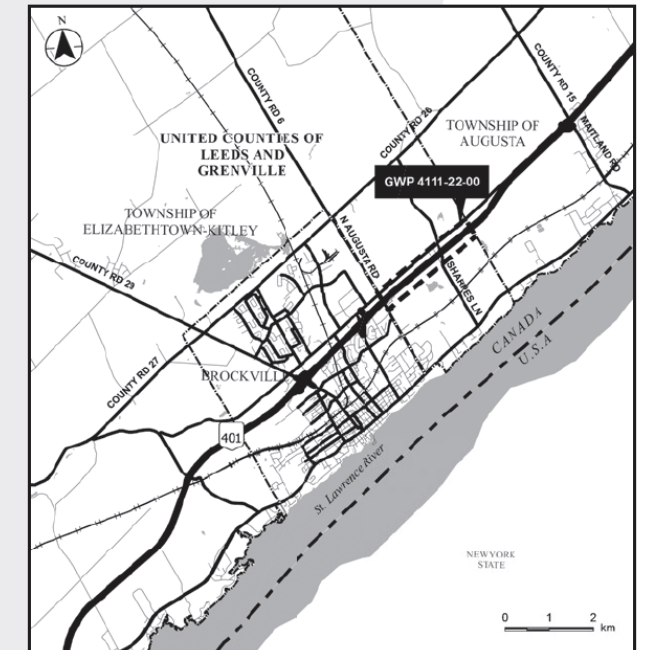
The second online PIC will be available for your review on the study website at www.hwy401eastofbrockville.ca starting July 17, 2025. A recorded presentation will be available as part of the online PIC until August 30, 2025.

COMMENTS

Additional details will be available on the project website, www.hwy401eastofbrockville.ca. We encourage you to participate in the study and to provide comments through the website or by contacting the project team members listed below. If you have questions about the study or would like to be added to the mailing list, please contact:

Gregg Cooke, P.Eng.
Consultant Project Manager
Stantec Consulting Ltd.
200-835 Paramount Drive
Stoney Creek, ON L8J 0B4
tel: 905-381-3227
call collect: 905-385-3234
e-mail: comments@hwy401eastofbrockville.ca

David Brake
Senior Project Manager
Ministry of Transportation Eastern Region
1355 John Counter Boulevard, Postal Bag 4000
Kingston, ON K7L 5A3
tel: 613-893-3031
fax: 613-540-5106
e-mail: comments@hwy401eastofbrockville.ca



If you have any accessibility requirements in order to participate in this project, please contact one of the Project Team members listed above. Comments and information are being collected to assist the MTO in meeting the requirements of the *Ontario Environmental Assessment Act*. Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.

Pour obtenir des renseignements en français, composer le +1 905 381-3224 (Tim Belliveau), courriel : tim.belliveau@stantec.com.

